

A Plan for Chicago's Near Northwest Side



City of Chicago
Richard M. Daley, Mayor
Department of Planning and Development
Alicia Mazur Berg, Commissioner

Skidmore, Owings & Merrill LLP
Urban Works Ltd.
Goodman Williams Group
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A Plan for Chicago's Near Northwest Side

Prepared by

*Skidmore, Owings & Merrill, LLP.
Urban Works Ltd.
Goodman Williams Group*

for

*The City of Chicago
Department of Planning and Development
and the
The Near Northwest Side Planning Advisory Committee*

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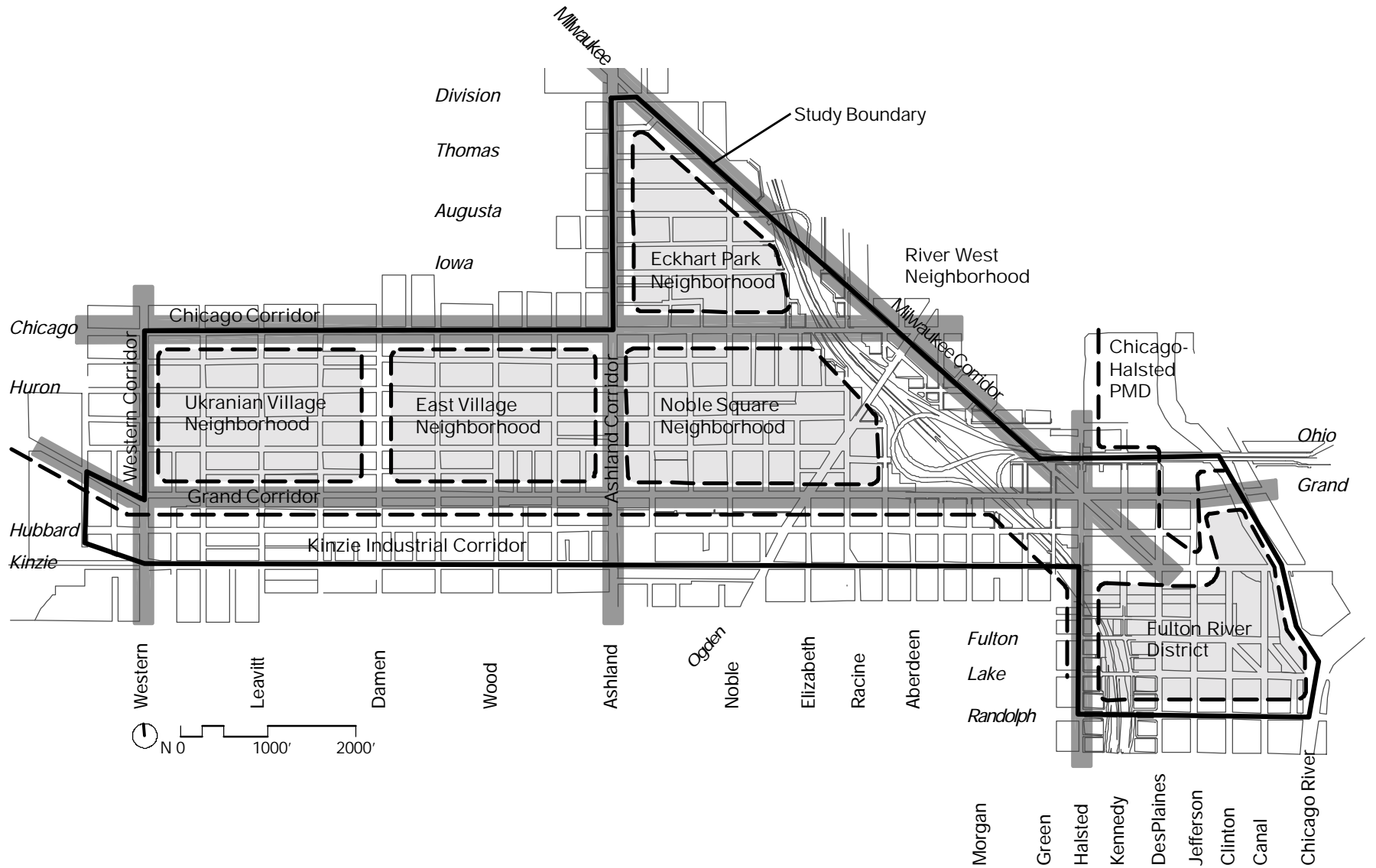
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Introduction

The Near Northwest Side encompasses the northwest Loop (Fulton River District), Eckhart Park, Noble Square, East Village and Ukrainian Village neighborhoods. The community touches the largest central business district in the Midwest to the southeast, and some of Chicago's most rapidly changing residential neighborhoods to the north. The Near Northwest Side is also home to the Kinzie Industrial Corridor and the Chicago-Halsted Planned Manufacturing District, which includes both older, established facilities and new high-tech operations.

The Near Northwest Side continues to evolve. The area includes the burgeoning residential and mixed-use Fulton River District, where residential conversions and new housing are being developed adjacent to industrial districts. New large retail developments are also being proposed. The neighborhoods in the west are seeing new townhomes, single family homes and loft conversions join the historic housing stock. The restaurant and entertainment boom is spreading north from downtown and south from Wicker Park, bringing new evening and weekend visitors to the area.

Why the Near Northwest Side Needs a Plan

New development and growth in the Near Northwest Side highlights a number of issues. Some represent long standing community concerns, while other issues have been raised as a result of new development. The issues include:

- Providing more open space and recreation amenities to accommodate long-time residents, new residents and future residents
- Improving the quality and capacity of transit and transportation infrastructure
- Guiding new development so it fits within existing neighborhoods and provides clarity to the develop-

ment community

- Constructing new buildings which properly address the street
- Creating new retail and commercial uses to meet the needs brought by new residential development.

To address these issues, the City of Chicago Department of Planning and Development initiated a process to engage community residents and leaders in the creation of a plan.

Creating a Plan for the Near Northwest Side

The planning process began with a series of neighborhood bus and walking tours with community residents and leaders in the Spring of 2000. Issues and opinions were discussed and local information shared. An Advisory Committee was formed, and over the following months it worked with the City of Chicago to:

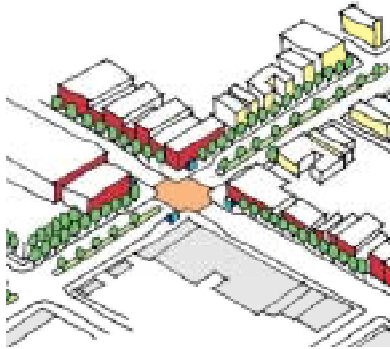
- Confirm neighborhood issues
- Establish guiding principles
- Develop recommendations for open space, transit and infrastructure improvements, neighborhood corridors and residential districts
- Organize wider community input on the recommendations developed.

This document represents the culmination of that process and provides a clear guide for the future growth of the Near Northwest Side for residents, for the City of Chicago, and for the development community. This document is also intended to be a working document over the 20 year life of the plan. Whereas, some recommendations may not come to fruition, new opportunities may arise and future recommendations should be reviewed within the framework of this plan.

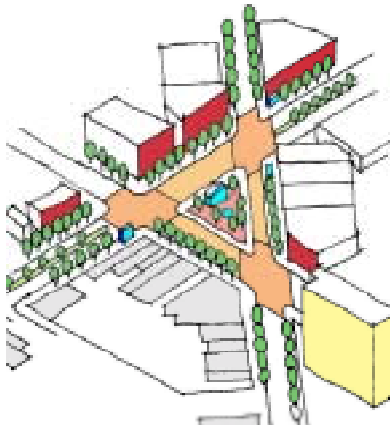


Executive Summary

Near Northwest Side



The Near Northwest Side's commercial "Main Street" along Chicago Avenue near Ashland Avenue will be strengthened.



Enhanced CTA Blue Line Station entrances will become the focus of public plazas along Milwaukee Avenue.

The Vision

The Near Northwest Side will continue to grow as a community of linked and distinctive urban neighborhoods. The community will be a vibrant place to live, offering a mix of uses, a full range of local amenities and clear connections to the rest of the city for work and play.

The Plan

The Plan's recommendations are organized in the following way:

- Completing the open space network
- Enhancing the transit system
- Guiding development in the Near Northwest Side
 - Strengthening neighborhood corridors
 - Promoting livable residential neighborhoods
- Next Steps: Implementing the Plan.

Completing the Open Space Network

Residents should be within a five minute walk of a neighborhood park. There should be clear connections to regional open spaces and large neighborhood parks via pedestrian-friendly and bicycle-oriented landscaped streets. New neighborhood open spaces should be created from available land within large planned developments, through the acquisition of new sites and the improvement of existing public facilities. Neighborhood schools should also serve as recreational resources. Key open space recommendations include:

- A publicly accessible riverfront
- New neighborhood parks
- School parks at neighborhood schools
- Upgraded neighborhood streets and dedicated bike routes
- New open spaces on underdeveloped land adjacent to infrastructure projects such as rail-lines and the expressway.

Enhancing the Transit System

Residents should be within a convenient walk of a CTA bus, CTA rail transit or Metra transit stop and adequate transit service should be provided. The streets, intersections, bridges and viaducts should be well-maintained and support the flow of traffic within the community. Key transit recommendations include:

- Potential new CTA Green Line stations
- Enhanced CTA Blue Line Station entrances
- Strengthened pedestrian connections between CTA and Metra Stations
- Improvements to bus stops such as new shelters, benches, signage and lighting.
- Implementation of the Central Area Plan transportation recommendations

Guiding Development in the Near Northwest Side

Strengthening Neighborhood Corridors

Strong neighborhood corridors will offer a mix of retail, entertainment, office, work spaces and residential uses. Neighborhood corridors will serve as, and be reinforced by, transit routes. Each corridor will offer active uses, physically and visually connected to the street. Key corridor recommendations include:

- Support Chicago Avenue as a neighborhood main street
- Support the Italian restaurant district and new mixed-use and residential uses on Grand Avenue
- Consolidate retail, improve transit facilities and upgrade the streetscape on Milwaukee Avenue
- Focus retail at major intersections and transit transfer points on Ashland Avenue
- Reflect the historic character of commercial corridor buildings in new developments.

Promoting Livable Residential Neighborhoods

Residential neighborhoods will have appropriately scaled and well designed residential buildings and homes. Key neighborhood recommendations include:

- The historic context of the East Village, Eckhart Park, Noble Square and Ukrainian Village neighborhoods should be respected by new development
- New development in the Fulton River District should respect the scale of existing buildings and the existing zoning regulations for a C3-5 district (the F.A.R. of seven should be maintained). Taller buildings should be located to the east of the District, which reflect heights in the Loop and along the Chicago River.

Next Steps: Implementing the Plan

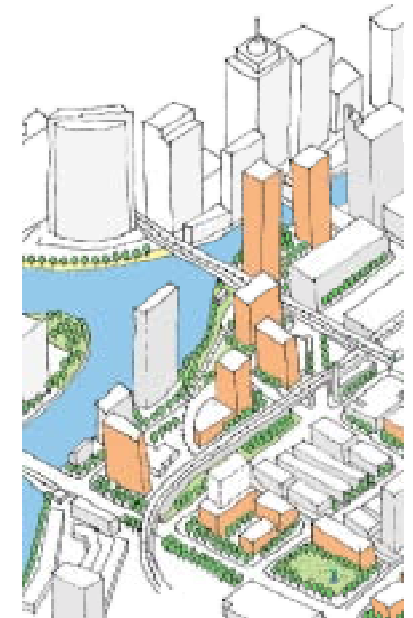
The plan outlines major activities, potential partners and potential funding strategies for the plan's recommendations. Key recommendations should be implemented within 5 years. Longer term recommendations may be implemented over the next 20 years.

Implementation will require the partnership of the City of Chicago's Departments of Planning and Development, Transportation, the Chicago Park District and Chicago Public Schools. Neighborhood residents, elected officials and local organizations will also be critical partners.

Many of these activities can be incorporated into the City's ongoing capital investment and development regulation activities. Others will require more directed initiatives, such as investment in new parks and open spaces and new transit facilities. Recommendations for new uses, densities and design requirements must feed directly into the efforts of the Mayor's Zoning Reform Commission.



A new gateway park and a potential civic institution at the intersection of Ogden and Grand Avenues will anchor a revitalized Grand Avenue 'Restaurant Row'



New riverfront access and neighborhood parks will enhance the livability of the Fulton River District



The Context

Understanding the Community

Origins of the Near Northwest Side



The Fulton River District in the 1940's - the intersection of Milwaukee and DesPlaines (looking southeast).



The historic core of the community developed along the streetcar lines - the intersection of Chicago and Ashland around 1920.

The Fulton River District

The Fulton River District is located on the edge of Chicago's downtown, northwest of the Loop. The district is bounded by the North Branch of the Chicago River, Halsted Street, Ohio Street to the north and Randolph Street to the south. James Thompson's original 1830 plat of Chicago was centered on Wolf Point at the fork of the Chicago River and included much of the area that is today the Fulton River District. The street grid and block layout imposed on this small area defined the pattern of Chicago's development as the city grew.

Commerce dominated the district for much of its history. Lumber and grain were shipped through the district, and *Sears and Roebuck's* first mail order warehouse was located at Fulton and DesPlaines. Randolph Street became the center of wholesale produce distribution in the late 19th Century and was the site of the famous Haymarket Square labor riots of 1886 on DesPlaines Street.

Historically, the district has also moved people. The City's first railroad terminal was built at Kinzie and Canal in 1848. Another terminal was built a few blocks south along the river and became Union Station. Today, the Ogilvie Transportation Center is a major commuter rail hub, and nearby connections to the Kennedy Expressway make this one of the most accessible places in the region.

Today, the Fulton River District serves as a transition from the Loop to the east to the lower density, residential neighborhoods of Eckhart Park, Noble Square, East Village and Ukrainian Village, to the west. Originally dominated by industry, warehousing and transportation, the Fulton River District is now increasingly residential. Low and mid-rise loft buildings are being converted to condominiums and apartments as well as offices. New residential development is bringing

townhomes, mid-rise and high-rise condos and apartments to the district. The district is also an entertainment venue, with nearby restaurants and nightclubs attracting visitors throughout the week. To the north, remaining industrial areas are defined by the Chicago-Halsted Planned Manufacturing District. This designation indicates the City of Chicago's commitment to retaining this area for industrial and commercial uses.

Eckhart Park, Noble Square, East Village and Ukrainian Village

To the west of the Fulton River District lie the historic neighborhoods of Eckhart Park, Noble Square, East Village and Ukrainian Village. These developed as working and middle class neighborhoods through the 19th and early 20th Centuries.

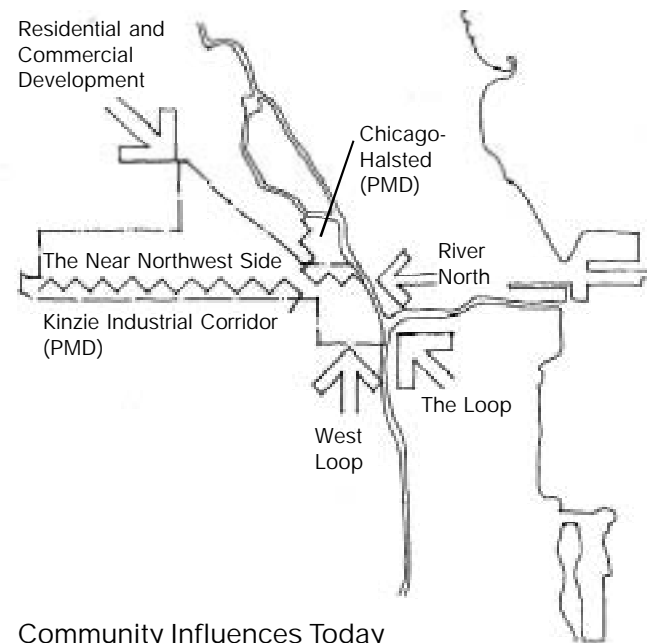
Residents came to these neighborhoods for the proximity to local jobs. Building out from what is now the Fulton River District, manufacturing and distribution operations moved up the North Branch of the Chicago River and west along the freight rail corridors. Residents could walk or ride streetcars to local jobs. Today, these industrial districts remain in the form of the Kinzie Industrial Corridor and the Elston and Goose Island Planned Manufacturing Districts.

Older homes can be found between Milwaukee Avenue, Ashland Avenue and Chicago Avenue. Worker's cottages, single family homes, two- and three-flats, corner walk-ups and storefronts with apartments above characterize the neighborhood. To the west of Ashland Avenue, two-flats and single family homes predominate.

Chicago Avenue developed as the "main street" for the community, offering a wide variety of stores and services that met community needs. Grand Avenue

functioned as a shopping and restaurant center between Halsted Street and Noble Street, but also included residents and workshop operations. More heavily influenced by neighboring industry, Milwaukee Avenue developed as a mix of workshop spaces, storefronts and apartments.

In recent years the same "back to the city" trend that has transformed the Fulton River District has begun to change Eckhart Park, Noble Square, East Village and Ukrainian Village. This process began with the renovations of older residential buildings in the early 1990's and then new construction later in the decade. New residents have impacted local commercial corridors, which have seen new shops, restaurants, bars and nightclubs emerge.



Community Influences Today



Northwestern University Settlement House on Augusta Blvd.



Goldblatt's department store at 1613-35 W. Chicago Avenue

The Historic Character of the Near Northwest Side

The Fulton River District and the Eckhart Park, Noble Square, East Village and Ukrainian Village neighborhoods have played a significant role in Chicago's social and economic development. This history is reflected in the traditional pattern of its streets and blocks and also in its residential, commercial and institutional building stock.

The City of Chicago has designated a number of neighborhood buildings as historic landmarks because of their particular importance to the life of the city. The Chicago Landmark Commission recommended landmark designation for the Ukrainian Village Landmark District at its September 2002 meeting.

The Northwestern Settlement House at 1400 West Augusta was one of the first institutions established in Chicago to provide the poor with educational and recreational programs.

Chicago Commons established social programs in the 1890's that the organization continues to provide to this day.

Goldblatt's department store on Chicago Avenue, built in the early 1920's, was the first in what became a regional chain and one of the nation's oldest large neighborhood department stores. It helped revolutionize American retailing. The building was renovated and the historic terra-cotta façade preserved by the Chicago Public Building Commission in 1996. It is now used as departmental offices for the City of Chicago.

In addition to the neighborhood's buildings, the Near Northwest Side has also been the locale for historic events.

On May 4, 1886 a decade of strife between labor and industry culminated in a confrontation at the corner of DesPlaines and Randolph Street that resulted in the death of both policemen and workers. This event is remembered across the world at May Day activities. A memorial to the Haymarket Tragedy and the loss of life that resulted is now planned close to the site of these significant events.

The historic character of these neighborhoods has played a significant role in attracting new residents to the area in recent years. Many residential buildings offer attractive opportunities for renovation, the landscape character of residential streets is appealing and commercial corridors offer stores, restaurants and transit service in close proximity to home. The history of the area is a major component of the quality of life it offers.

The City of Chicago will continue to protect and preserve the most significant buildings and districts through landmark designation.



There is a rich historic building stock throughout the Near Northwest Side's Neighborhoods.



Historic warehouse buildings along Randolph Street.

Open Space

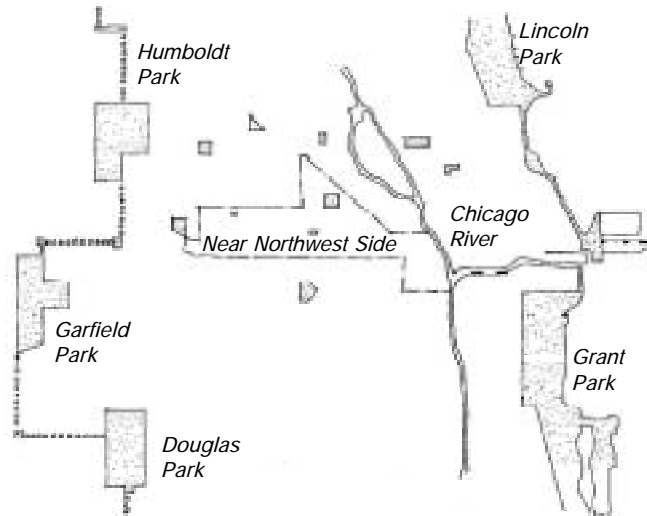
The Near Northwest Side lies between Chicago's two main open space resources: the lakefront and the necklace of regional parks linked by the Boulevard system. The Chicago River increasingly offers public access and recreational opportunities. The Near Northwest Side itself is also home to Eckhart Park and is bounded on the west by Smith Park. Both are larger Chicago Park District parks offering a variety of recreational activities, in both outdoor spaces and field house facilities.



Community facility at Eckhart Park



Recreational facility at Smith Park



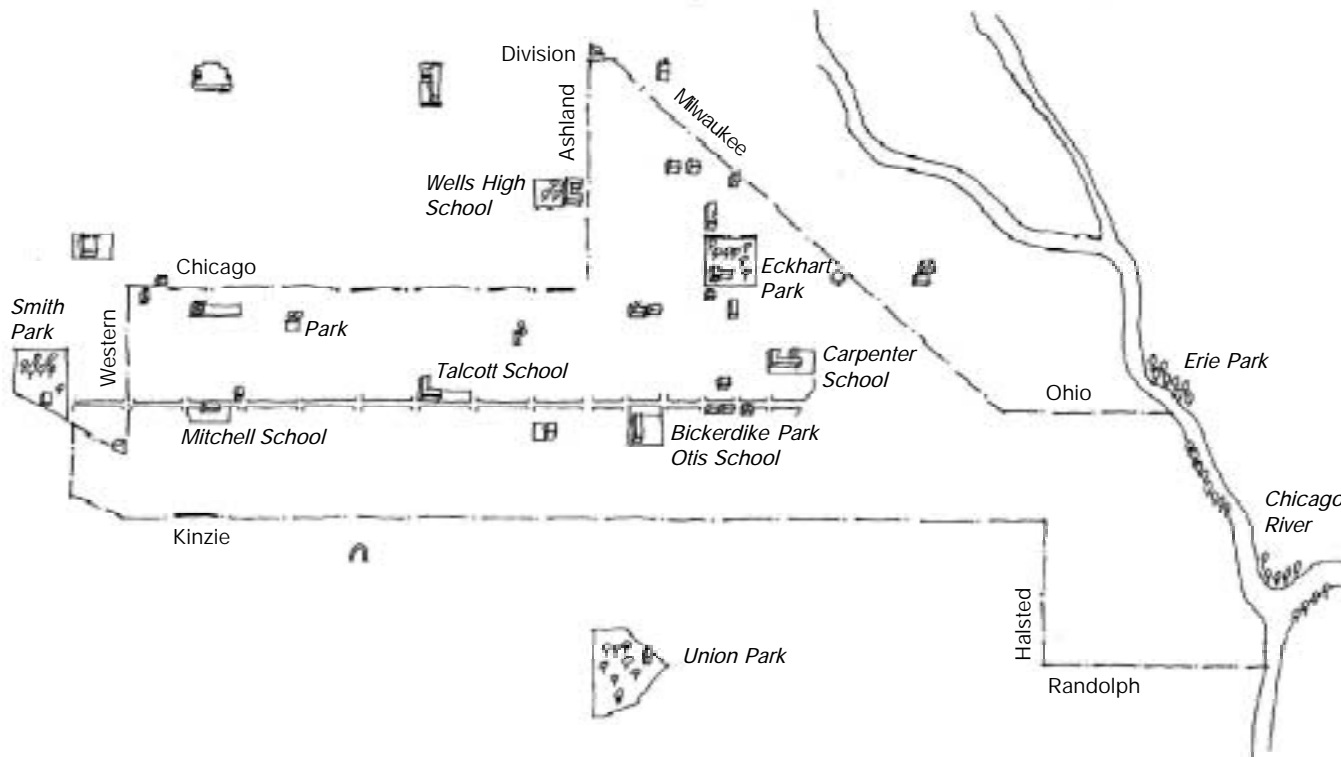
In addition, the neighborhood offers a series of smaller parks and school campus recreation facilities such as Bickerdike Park and Talcott, Otis and Carpenter Elementary schools.

However, the residents have expressed concerns that the community lacks adequate open space and park amenities.

- In the Fulton River District, public open space and park amenities have not always been offered in many of the new residential projects.
- Parks and open spaces have not yet been clearly defined for all future developments, in anticipation of the influx of residents.
- Much of Noble Square and East Village were also built without parks or open space.
- Residents feel that the neighborhood must offer more open spaces and recreation opportunities for play, exercise, dog walking and relaxation.
- Open spaces called for include parks offering active and passive recreation opportunities, urban plazas, community gardens and improvements to spaces adjacent to highway, railroad or utility infrastructure.

Regional Open Space

Strong physical connections to the regional park system, the river and the lakefront are lacking.



Neighborhood Open Space and Schools



The Otis School



A neighborhood park in Ukrainian Village

Near Northwest Side



Lake Street Green Line



Milwaukee Avenue Blue Line



Transit Plaza - Division and Ashland

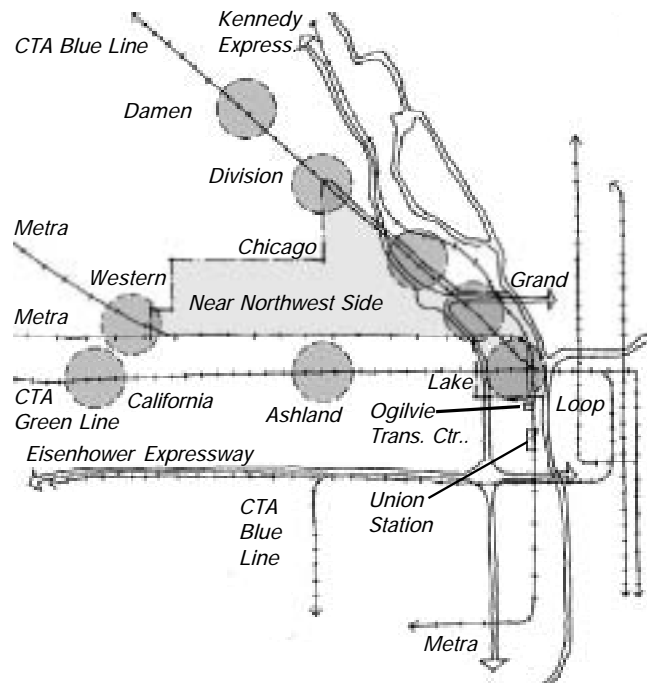


Chicago Avenue Bus Stop

Transit and Infrastructure

The Near Northwest Side lies close to the center of the regional transportation system. Regional rail connects to Union Station and the Ogilvie Transportation Center. The Chicago Transit Authority Rapid Transit Blue Line runs through the Milwaukee Avenue Corridor to the Loop.

The Chicago Transit Authority Rapid Transit Green Line runs through the Fulton River District connecting the West Side with the Loop. Major streets also offer CTA Bus Service.

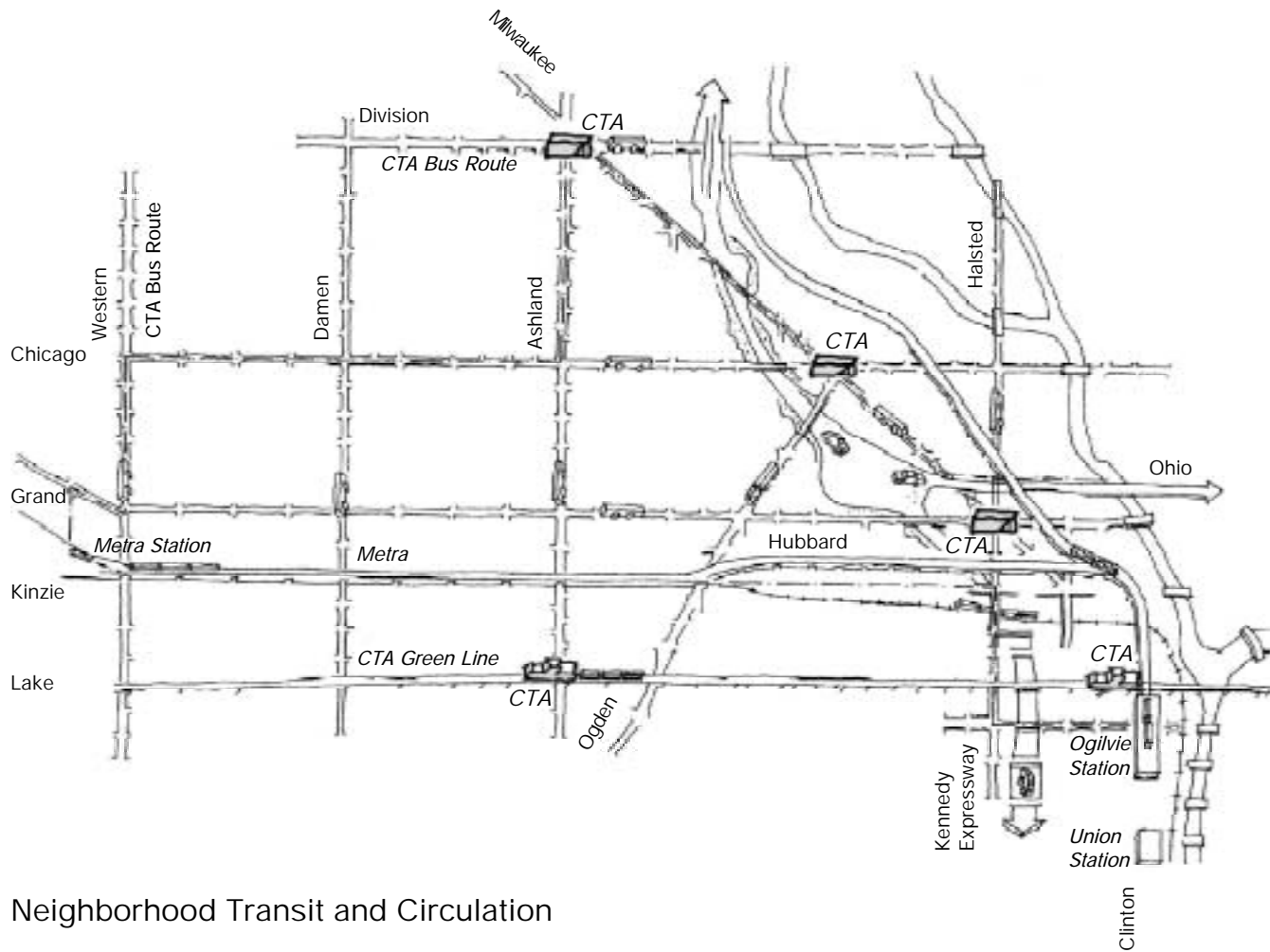


Regional Circulation

The Kennedy Expressway runs through the Near Northwest Side, and offers numerous connections to the Fulton River District, Eckhart Park, Noble Square, East Village and Ukrainian Village. Ashland Avenue, Ogden Avenue, Western Avenue and Halsted Street serve as community connectors to other Chicago neighborhoods. Chicago Avenue, Grand Avenue and Milwaukee Avenue offer links to other neighborhoods but also serve as community commercial corridors.

While the Near Northwest Side is well-served from a transit and transportation perspective, the community does have some concerns.

- Physically, train stations in this area are far apart: there is no Metra Station between Western Avenue and the Loop, and no Green Line Station between Clinton Street and Ashland Avenue.
- Blue Line Station entrances are not felt to be sufficiently welcoming or visible.
- Not enough bus stops have shelters.
- Links between transit modes are not fully developed, and transfer points and route intersections are not clearly identified.
- Commercial corridor streetscapes need to be upgraded.
- The pedestrian quality of streets in the Fulton River District is undermined by poor sidewalk, crosswalk, at-grade rail crossing and viaduct lighting conditions.
- The sidewalk and streetscape conditions in Eckhart Park, Noble Square, East Village and Ukrainian Village vary in quality and maintenance.
- At grade rail crossings in the Fulton River District are not well maintained and create conflicts with pedestrians and cars.



Neighborhood Transit and Circulation



The Kennedy Expressway



The Union Pacific Viaduct along Hubbard Street

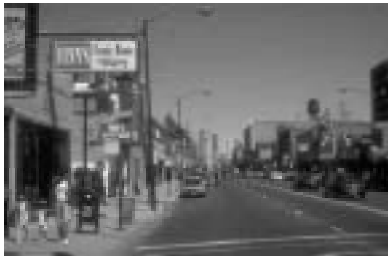


Vacant property along the Union Pacific Viaduct near Kinzie Street

Near Northwest Side



Chicago Avenue



Chicago Avenue



Milwaukee Avenue



Randolph Street

Development Corridors

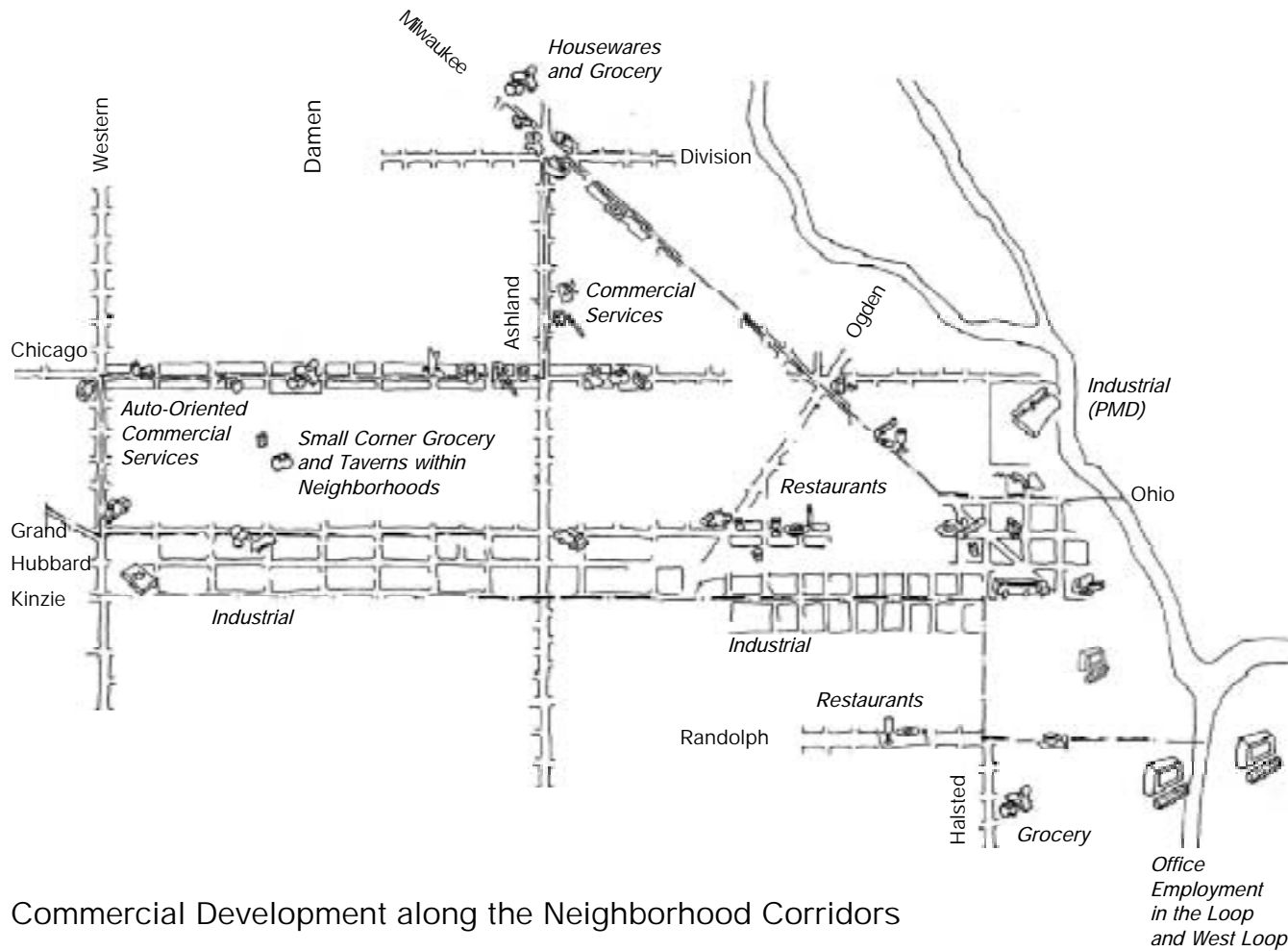
Eckhart Park, Noble Square, East Village and the Ukrainian Village residential areas are organized within a clear framework of development corridors.

- Chicago Avenue is the neighborhood's main street with a mix of local convenience retail and services, municipal offices, Mexican restaurants, new boutiques and Ukrainian bakeries and restaurants. It is also attracting new independent boutique fashion and furniture stores.
- Grand Avenue connects the West Side to downtown. It is a neighborhood retail district at the east, a residential street around Ashland Avenue and a light industrial and workshop corridor to the west.
- Milwaukee Avenue connects the Northwest Side to downtown. It is home to workshop spaces, affordable housing, new housing development, arts spaces, renovated storefronts and convenience retail.
- Ashland Avenue is a major arterial street, running through the neighborhood to the north and south sides of the city. Retail, businesses, housing and schools line the street.
- Western Avenue is a major arterial street, running through the neighborhood to the north and south sides of the city. Western Avenue has industrial businesses, lofts and local convenience retailers.
- Ogden Avenue is the gateway to the neighborhood from the Kennedy Expressway. The boulevard also connects the neighborhood to the Near West Side and functions primarily as a community connector rather than a major development corridor.
- Halsted Street is a major north-south arterial street. Halsted Street has industrial businesses, lofts, restaurants and local convenience retailers.
- The Kinzie Industrial Corridor continues to operate as an active industrial corridor. The ten largest firms in the corridor account for more than 2,000 jobs.

Today, the Near Northwest Side is confronted with a number of issues relating to its development corridors. Although there have been some recent investments in new retail or residential buildings, the development corridors have yet to see the renovations or new development found in the residential districts themselves. There are also concerns that new retail development will come in the form of strip malls or other auto-oriented businesses that will detract from the historic character of the neighborhood.

- Chicago Avenue has storefront and upper floor vacancies, particularly at the periphery east of Ashland Avenue and west of Damen Avenue.
- The Grand Avenue restaurant district does not have enough city-wide and regional visibility, and there are gaps in street level activity elsewhere.
- Milwaukee Avenue is fragmented by bridges over the Kennedy Expressway, which runs beneath it at Augusta and the Ontario-Ohio interchange. It offers homes, work spaces, restaurants, and convenience retail but still lacks a clear identity, or a way of organizing development or pedestrian activity on the street.
- Ashland Avenue has vacant storefronts, poor quality buildings and facades and poor sidewalk conditions, particularly between Grand and Chicago Avenues.
- Ogden Avenue has vacant buildings and underutilized land.
- Halsted Street is fragmented by bridges over the Kennedy expressway and Union Pacific tracks.
- The influx of an estimated 4,000 residents into the Fulton River District into a previously industrial area has yet to bring daily convenience retail or create identifiable commercial corridors. The increase in residential population and the expanding office core create an opportunity for new retail in the district.

*The Context
Understanding the Community*



Commercial Development along the Neighborhood Corridors



Ogden and Grand Intersection



Grand Avenue



Ashland Avenue



Ashland Avenue

Near Northwest Side



Single family development



Single and multi-family mix



Corner Lot Development



Multi-family flats

The Neighborhoods

The Fulton River District

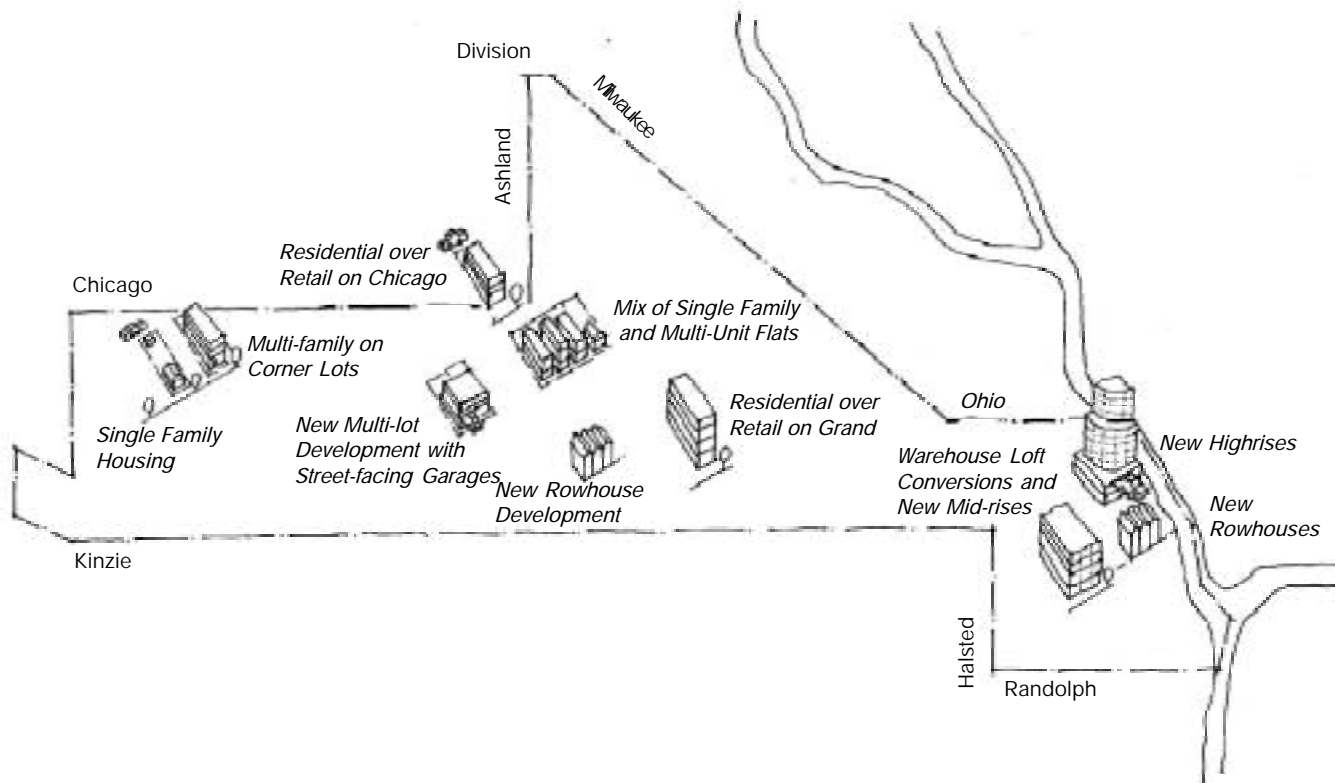
The neighborhoods of the Near Northwest Side begin with the higher-density, larger-lot Fulton River District to the east. Building heights reflect proximity to downtown and the area's history as an industrial loft district. However, there is also great contrast in scale, with townhomes next to twenty-story towers. This new residential neighborhood faces numerous issues.

- The height and scale of new buildings is not organized in a clear way.
- Proposed super-block developments do not create through-streets or pedestrian routes.
- Buildings adjacent to the bridges over the Metra tracks have limited access to the sidewalks and present blank walls along the street.
- New building entrances and windows are directed towards the interior of developments creating blank building facades on neighborhood streets.
- In some cases, garages, driveways and curb cuts line sidewalks, undermining the pedestrian environment.
- The Fulton River District needs retailers that can offer food service, grocery stores, clothing stores, and book and video stores to support both daytime workers and residents.
- There are potential conflicts between historic industrial uses north of Kinzie Street and new residential development in the area.

Eckhart Park, Noble Square, East Village and Ukrainian Village

Set between mixed-use corridors, Eckhart Park, Noble Square, East Village and Ukrainian Village were each built on the traditional Chicago street grid, block and alley pattern. Lots are typically organized on standard Chicago dimensions of 25' by 125'. Residential building types include bungalows, single family houses, two- and three-flats and some four story walk up apartment buildings. Townhouse, three-flat and six-flat condominium buildings are common building types among new developments. The influx of new residents, renovation and new development has raised a number of issues.

- The scale of new residential buildings has often been out of scale with those of neighboring homes.
- There are concerns that entries, doors, windows and facades do not reflect the historic character of the neighborhood or good urban design principles.
- Some new projects have placed garage doors and curb cuts at the sidewalk, undermining the pedestrian quality of local streets.
- The local stand-alone convenience stores that were often found on residential streets are disappearing.



Building Types and Locations in the Neighborhood



Loft Development



High-rise development

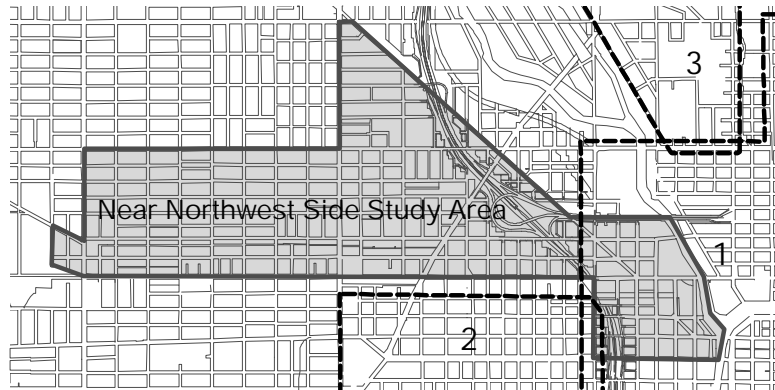


Inappropriate development with a garage facing the street



Inappropriate new development that does not face the street

Near Northwest Side



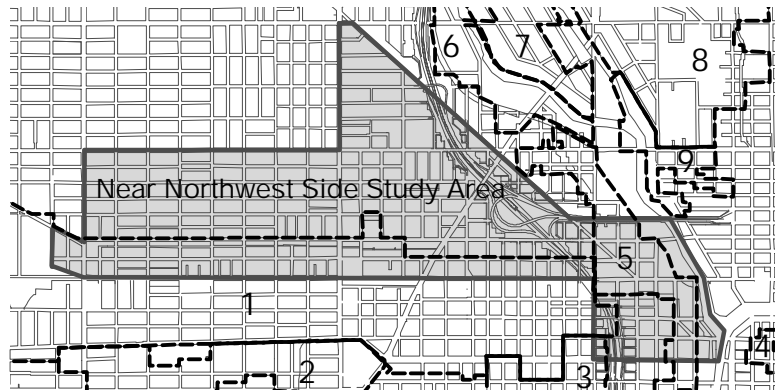
Plans Completed or in Progress

- 1 Central Area Plan
- 2 Near West Side Area Land Use Plan
- 3 Cabrini-Green Redevelopment



Industrial Districts

- 1 Goose Island
- 2 Elston Industrial Corridor
- 3 Kinzie Industrial Corridor
- 4 Chicago-Halsted P.M.D.



T.I.F. Districts

- 1 Kinzie Industrial Corridor
- 2 Central West
- 3 Near West
- 4 Central Loop
- 5 River West
- 6 North Branch South
- 7 Goose Island
- 8 Near North
- 9 Chicago Kingsbury

Planning Area Summary

The City is engaged in numerous planning efforts which may influence development in the Near Northwest Side. The maps to the left identify the relevant completed and in-progress plans, industrial districts and T.I.F. districts.

Existing Zoning

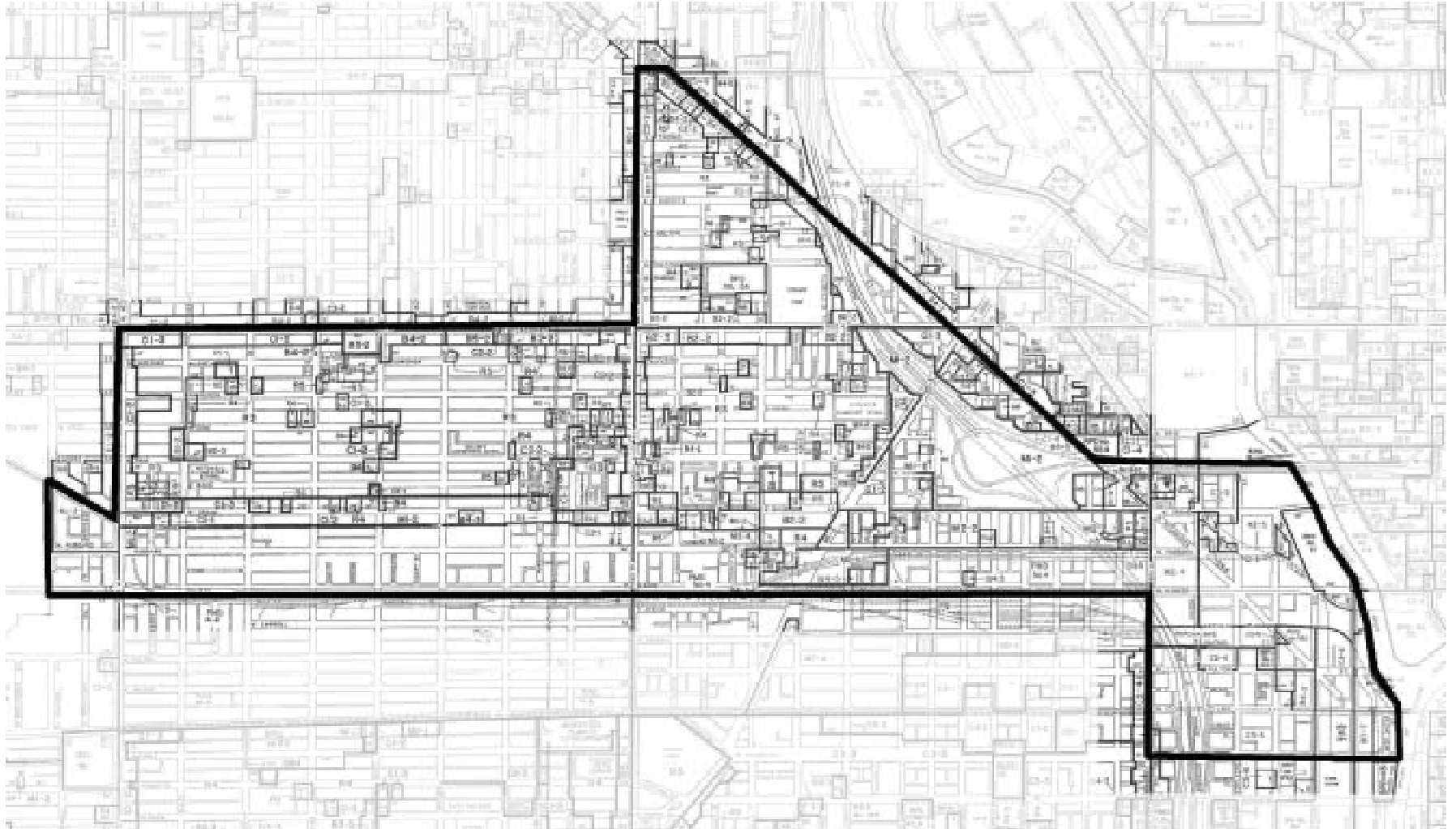
The zoning map for the Near Northwest Side is included on the facing page. The allowable land uses are determined by 4 categories: Residential (R), Business (B), Commercial (C) and Manufacturing (M). The allowable build-out is determined by Floor Area Ratios (F.A.R.). The F.A.R. for a site is based on the lot size. For example, the F.A.R. for a lot in the R3 zone is 0.9. If a lot is 25' by 100', the lot size is 2500 square feet. The allowable build-out for the lot would be 2500 x 0.9 for a total of 2250 square feet of building space.

Zoning Map Summary

<u>Residential Districts</u>	<u>F.A.R.</u>
R1 Single Family	0.5
R2 Single Family	0.65
R3 General Residence	0.9
R4 General Residence	1.2
R5 General Residence	2.2
R6 General Residence	4.4
R7 General Residence	7.0
R8 General Residence	10.0

Business Districts (B1 thru B7), Commercial Districts (C1 thru C5), Manufacturing Districts (M1 thru M3)

	<u>F.A.R.</u>
B/C/M -1	1.2
B/C/M -2	2.2
B/C/M -3	3.0
B/C -4	5.0
B/C -5	7.0
B -6	12.0
B -7	16.0



Existing Zoning

The City of Chicago Zoning Ordinance and Map may be found on the internet at <http://w5.ci.chi.il.us/Zoning/Zoning.html>.



The Plan

A Vision for the Near Northwest Side

The Near Northwest Side will continue to grow as a community of linked and distinctive urban neighborhoods. The community will be vibrant place to live, offering a mix of uses, a full range of local amenities and clear connections to the rest of the city for work and play.

There will be clear connections to regional open spaces and large neighborhood parks via pedestrian-friendly and bicycle-oriented landscaped streets. New neighborhood open spaces will be created from available land and as major new developments occur. Neighborhood schools will also serve as recreational resources. Open lands associated with highway and rail infrastructure will be upgraded to add to the landscape quality of the neighborhood.

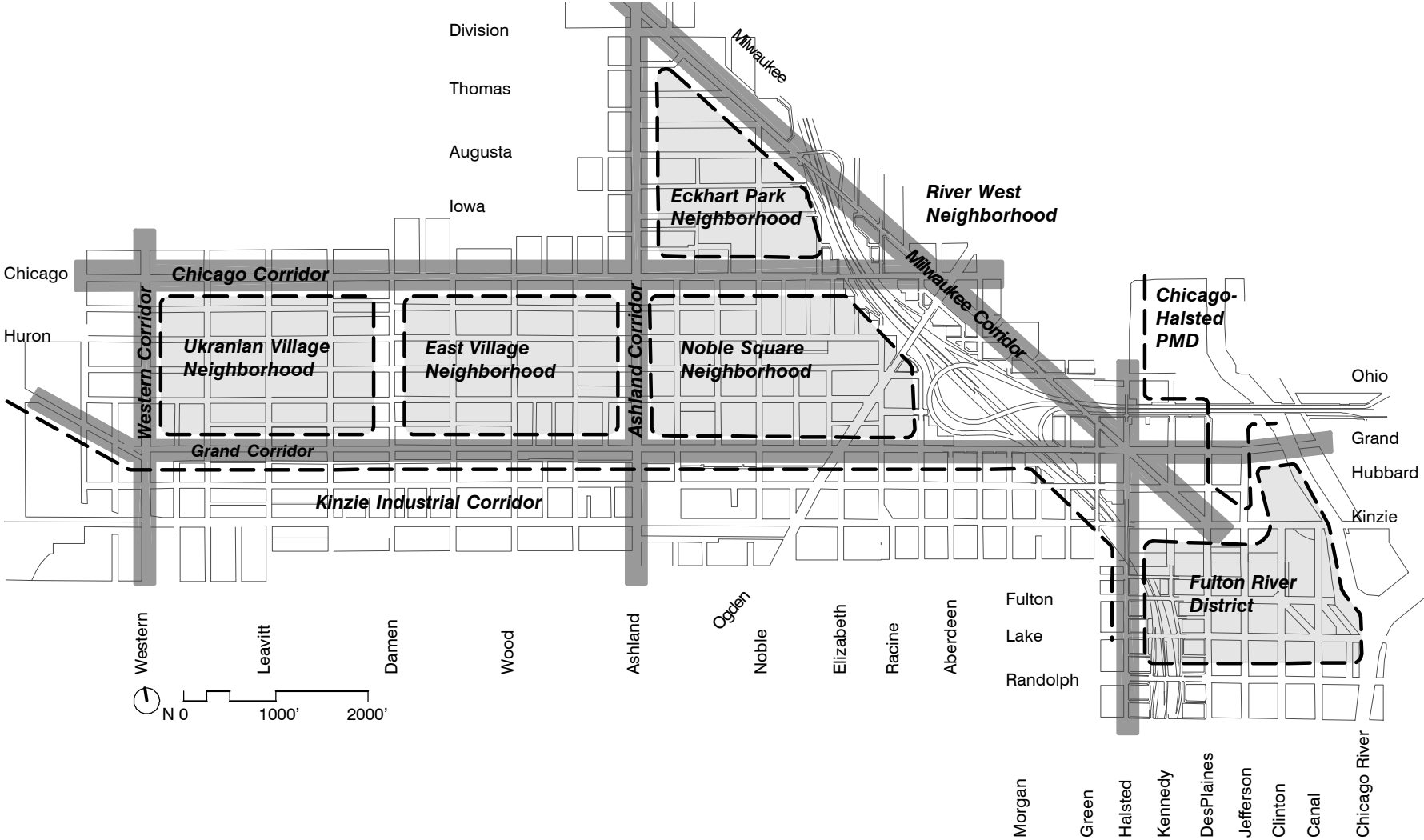
The neighborhoods will be linked to each other with corridors that offer a mix of retail, entertainment, office and residential uses. Each corridor will offer active uses, physically and visually connected to the street. Neighborhood corridors will serve as, and be reinforced by, transit routes. Streets will offer high quality pedestrian environments through the appropriate design of streets and the buildings that line them. Where corridors connect, they will be marked by high quality streetscapes, transit facilities and a cluster of active street-level uses.

The Fulton River District will grow into a high quality urban neighborhood. The scale of new residential buildings will be greater than elsewhere on the Near Northwest Side. With appropriate design, a mix of uses, a high quality pedestrian environment and public amenities, a strong residential community will be created.

Ukrainian Village, East Village, Eckhart Park and the Noble Square Neighborhoods will continue to be strong residential areas, offering a mix of appropriate housing types, high-quality housing and street environments. The neighborhoods will have appropriately scaled and well-designed residential buildings and homes. People will be able to see onto streets from their homes and passers-by will be able to see into homes, creating a visual connection between them and putting “eyes on the street”. The preservation and renovation of historic buildings will be encouraged. Parking access will be provided from alleys or the rear of buildings.

Industrial corridors will remain viable places to do business, whether manufacturing or for distribution of high tech operations. Many neighborhood and Chicago residents will continue to hold jobs in these districts.

Corridors and Neighborhoods



Completing the Open Space Network

Guiding Principles

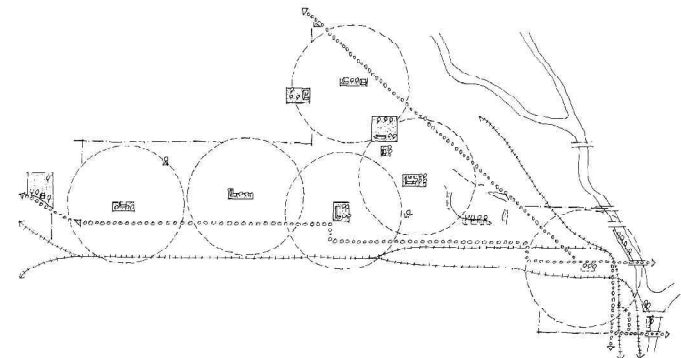
The following principles will guide the development of open space and recreation amenities in the Near Northwest Side.

- Encourage an open space network in the Near Northwest Side Neighborhood.
- Promote access to the Chicago River and the Lakefront.
- Promote access to regional parks such as Smith, Humboldt and Eckhart Park.
- Encourage the use of neighborhood schools for open space and recreation resources.
- Enhance the landscape quality of key pedestrian streets linking neighborhood and regional open spaces.
- Encourage public parks and plazas as part of new larger scale residential developments.
- Encourage the inclusion of landscaped private open spaces that are visible from streets in new residential development projects.
- Incorporate landscaping into infrastructure such as viaducts and expressway edges and interchanges.
- Promote a diversity of open spaces for adults, children and their pets.

Recommendations

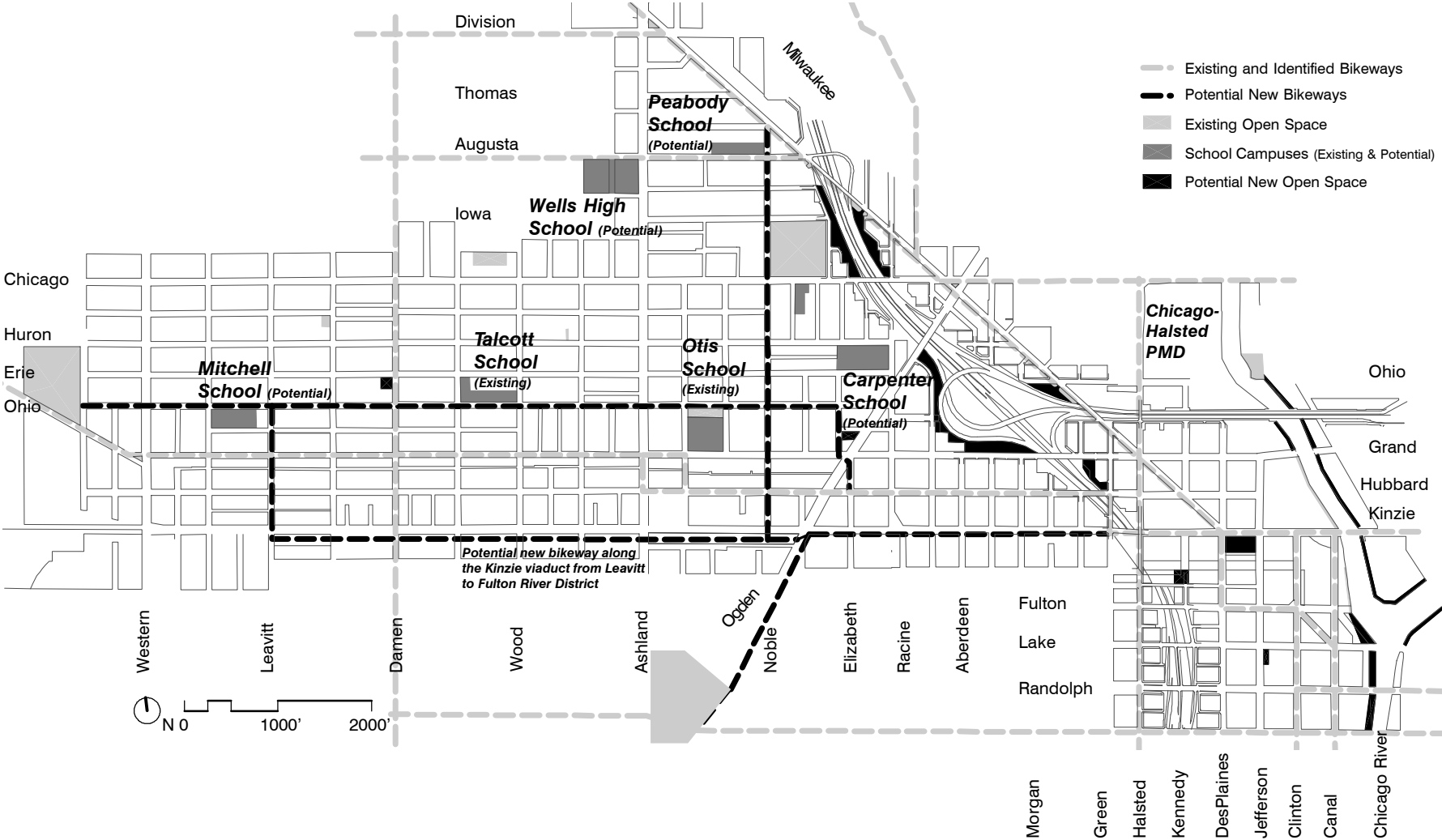
New Open Spaces

- Create new publicly accessible, landscaped riverfront greenways as new development occurs adjacent to the Chicago River. Public access to the river edge and links between existing segments should also be created as new development occurs.
- Create a new neighborhood park at the southeast corner of DesPlaines Street and Kinzie Street as part of the Kinzie Station Planned Development.
- Create a new neighborhood open space south of the Union Pacific tracks at Union Street.
- Create an urban plaza in the core of the Fulton River District in the vicinity of Lake, Jefferson and Clinton. Potential sites include the southeast corner of the Lake and Jefferson Streets intersection or the southwest corner of the Lake and Clinton intersection.
- Improve the recreational facilities of school parks at Mitchell, Carpenter, Peabody and Wells Schools. Otis and Talcott have been completed.

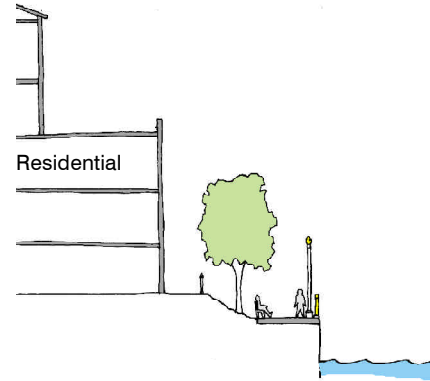


Residents should be within a five minute walk of a well-maintained neighborhood park.

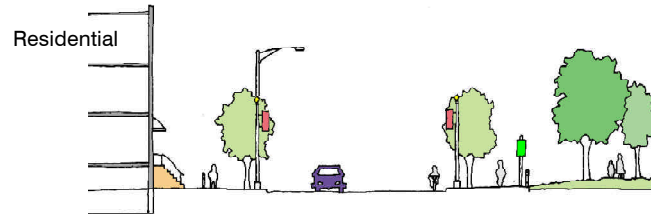
The Open Space Network



Near Northwest Side



New development adjacent to the river should provide a high-quality linear greenway.



Neighborhood streets, Ohio and Noble, should be enhanced with bike routes, lighting and signage to create strong links between neighborhood parks and open space.

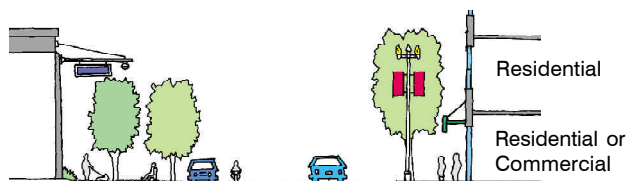
New open spaces (continued)

- Encourage a new gateway park on the vacant land at the northwest corner of Ogden Avenue and Grand Avenue.
- Encourage a new park at the southwest corner of Erie Street and Damen Avenue.
- Promote new open spaces on underdeveloped or remnant land adjacent to the Kennedy Expressway and the Ohio Feeder between Augusta and Grand. Potential improvements include dog-walking spaces and a sculpture garden adjacent to Grand Avenue.
- While Commercial Park lies just beyond the study area, consideration should be given to enlarge the park at Rice and Wolcott so the park has direct frontage on Chicago Avenue.
- Create a new pedestrian and bicycle open space connection on the rail right-of-way along Kinzie Street, linking Leavitt Street to the Fulton River District.

Access to Regional Open Spaces

- Maximize the visibility of designated and proposed bikeways on Grand Avenue, Hubbard Street, Milwaukee Avenue and Kinzie Street with bike lane striping, signage, lighting and landscaping.

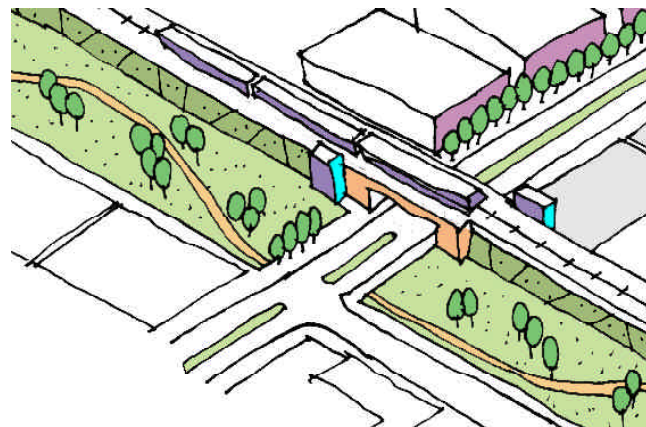
- Upgrade the landscape character of Noble Street and Ohio Street to create strong pedestrian and bicycle links between neighborhood parks and open spaces.
- Add directional signage to street connections to the Chicago River and the lakefront from the Near Northwest Side.
- Slow traffic at neighborhood schools.



The Union Pacific Viaduct along Hubbard and Clinton may be developed as a linear park.

Private Open Space in New Development

- Chicago has a strong tradition of front yards and courtyard entries to residences that are private, yet still contribute to the landscape quality of neighborhood streets. Ensure that private open spaces that are created in new developments, such as landscaped entries and courtyards, are visible from local streets.
- Private landscaping such as trees, shrubs, and flowers adjacent to and visible from neighborhood streets is encouraged. Also, the inclusion of ground cover and shade trees can help minimize the urban heat-island effect.
- Maximize landscape opportunities in private developments. Encourage the use of green roofs for private open space.



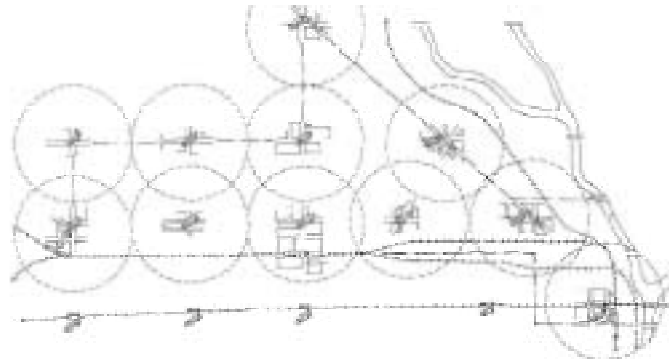
A linear park with pedestrian and bicycle pathways may be developed along the Kinzie railroad right-of-way.

Promoting A High Quality Transit System

Guiding Principles

The following principles will guide the development of transit and infrastructure in the Near Northwest Side.

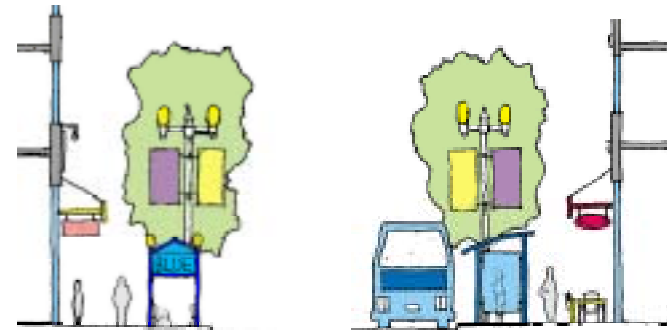
- Ensure that the Near Northwest Side offers a variety of convenient transportation alternatives.
- Ensure that adequate transit service is provided.
- Ensure that streets, intersections, bridges and viaducts are maintained and support the flow of traffic through the neighborhood.
- Ensure clear truck route access to industrial businesses.
- Extend Chicago's Bike Route network through the neighborhood.
- Ensure that new development, especially larger development projects, maintain the block and street patterns of the neighborhood.
- Use street and building design to minimize negative impacts caused by bridges, viaducts and rail.



Residents should be within a convenient walk of a CTA Bus, CTA Rail Transit or Metra Transit stop.

Recommendations

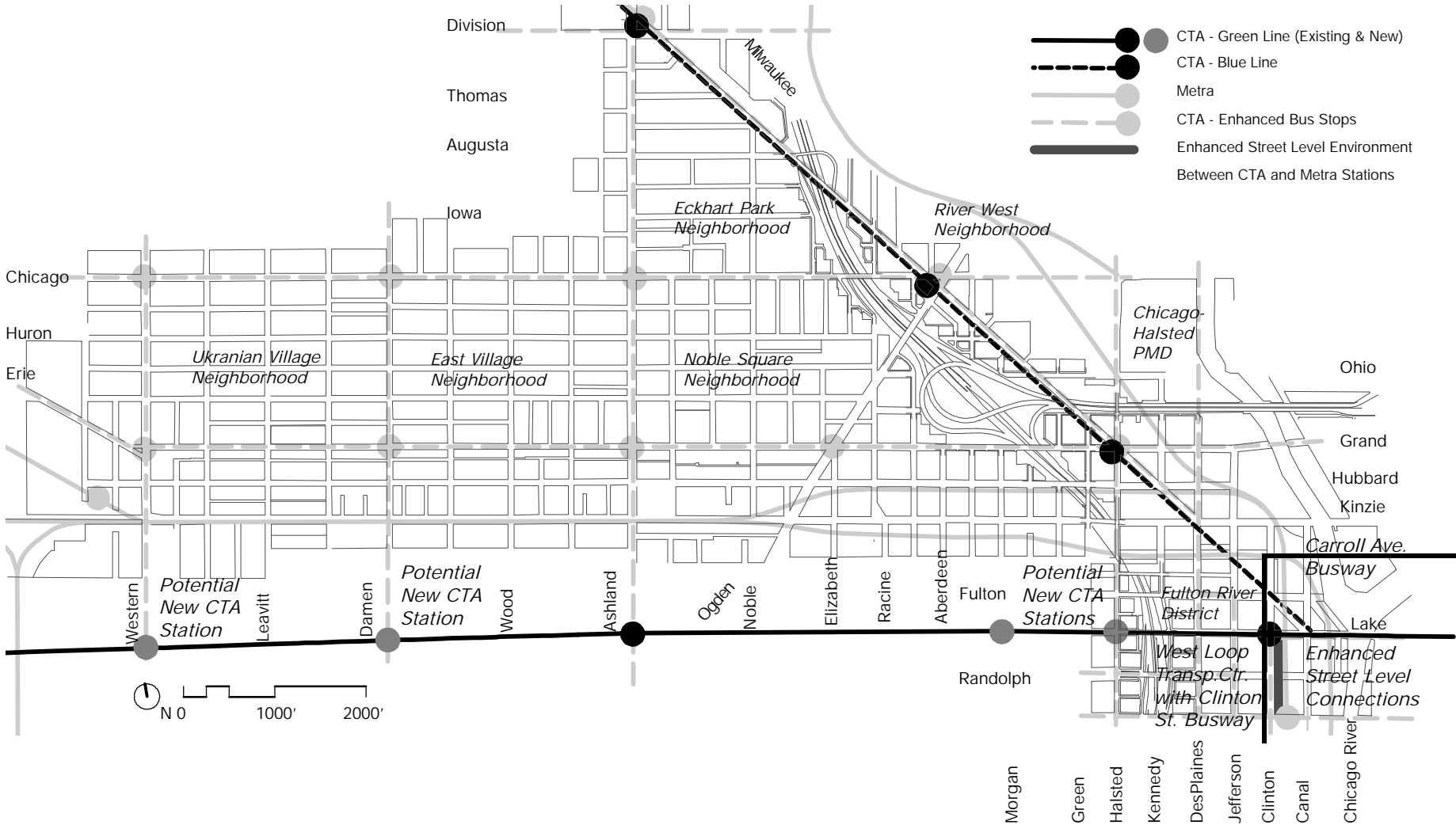
- Assess CTA usage on the Green Line and develop recommendations for new stations. Potential locations are Western Avenue, Damen Avenue and either Morgan Street or Halsted Street.
- Upgrade entrances to Blue Line Stations through maintenance, redesign, new signage and lighting.
- Improve the pedestrian environment along Clinton Street between the Ogilvie Transportation Center and the Clinton Street Green Line station.
- Improve bus stops at all quarter mile streets and bus-route intersections by improving sidewalks and lighting and adding shelters, benches and providing clearly visible signage.
- Improve bus signage throughout the community.
- Improve CTA service hours to reflect areas's demand.
- Improve at-grade railroad crossings.
- Implement Chicago's Central Area Plan, including:
 - West Loop Transportation Center on three-levels below Clinton Street with CTA subway, commuter rail and Clinton Street Busway.
 - Carroll Avenue Busway: an exclusive busway using existing rail rights-of-way to connect Union and Ogilvie Stations with River North and Streeterville.



Enhance subway entrances along Milwaukee Avenue.

Improve bus stops to include shelters, lighting and informational signage.

The Transit System



Guiding Development

New development in the Near Northwest side will be directed in a way that maintains and strengthens the historic framework of urban corridors and residential districts. New development should be consistent with City Ordinances and Guidelines. Relevant City Ordinances include: Parking Garage, Townhouse, Landscape, Strip Mall and Drive-thru Regulations. Relevant Guidelines include: Neighborhood Commercial Corridor Guidelines, Riverfront Guidelines, Open Space Guidelines and CTA's Transit Oriented Development Guidelines.

Strengthening Neighborhood Corridors

Guiding Principles

The following principles will guide the development of neighborhood corridors in the Near Northwest Side.

- Support strong mixed-use corridors with retail, office, workshops and housing providing an active street orientation.
- Encourage higher density and mixed use development on commercial streets and close to transit stations and intersections.
- Consolidate retail at commercial street intersections and close to transit stations.
- Ensure that neighborhood-scale retail is available for residents within a convenient distance from home.
- Support the development of the Chicago-Halsted Planned Manufacturing District and the Kinzie Industrial Corridor as a place for production and distribution.

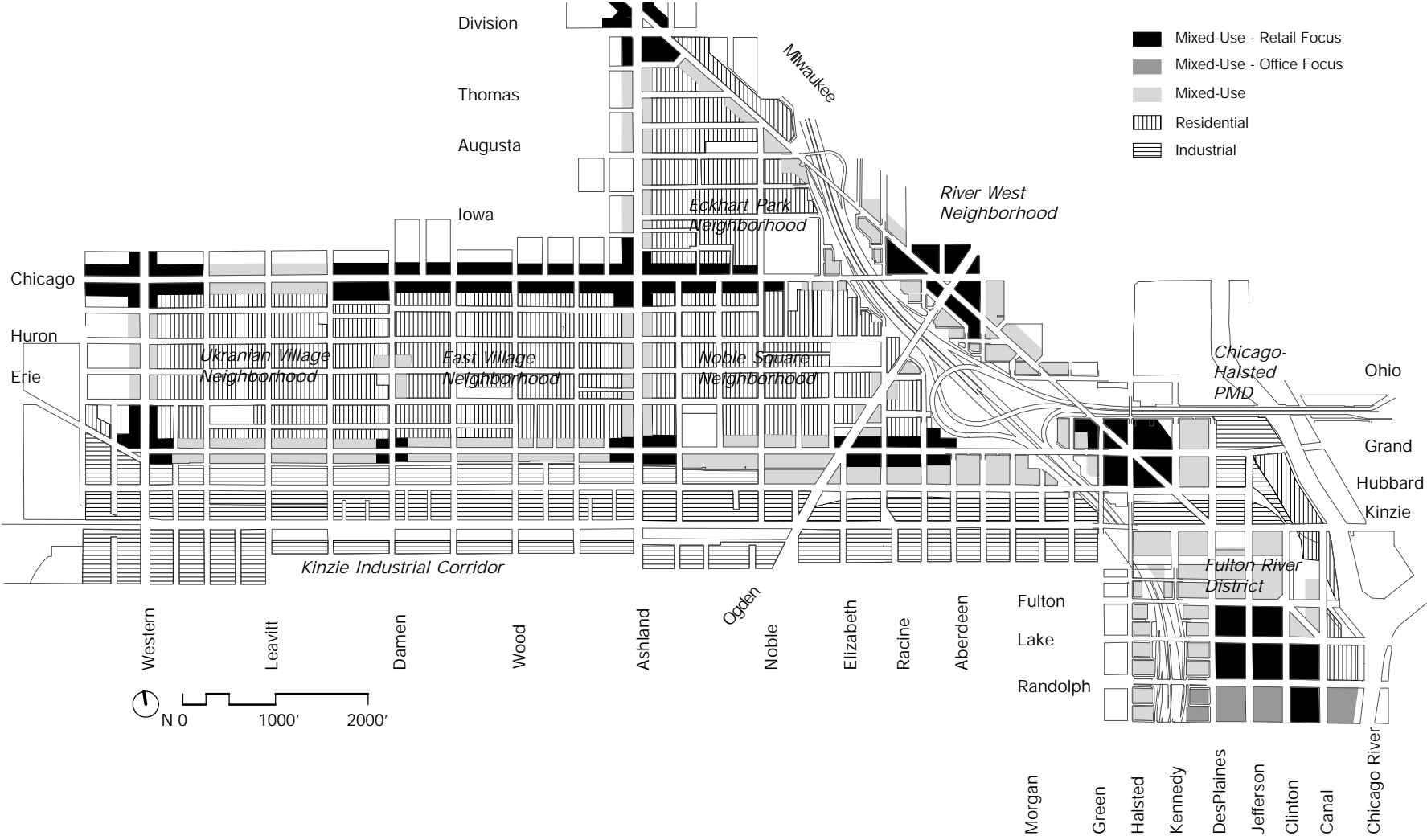
Promoting Livable Residential Neighborhoods

Guiding Principles

The following principles will guide development in the Near Northwest Side neighborhoods.

- Maintain the neighborhood's mix of housing, retail, industrial, commercial and institutional uses.
- Encourage the development of a mix of housing types and tenures in residential neighborhoods.
- Respect the historic development character of sub-areas with regard to building scale, orientation and setbacks.
- Support the development of local institutions such as schools, churches and neighborhood settlement houses.
- Explore the development of a new regional library.
- Work with the community and developers to reinforce a pedestrian-friendly and safe environment. Potential improvements include enhanced lighting and traffic-calming measures.

Development in the Neighborhood



Strengthening Neighborhood Corridors

Street Character Framework

The Street Character Framework establishes a hierarchy of streets that may be used to guide development and streetscape improvements.

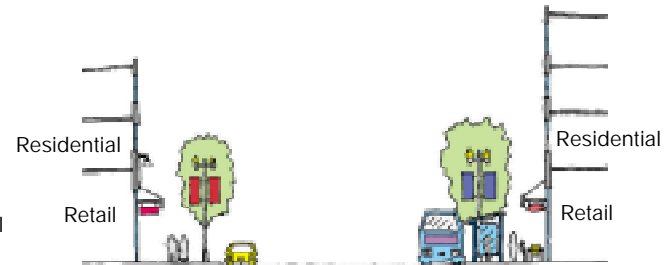
- *Community Commercial* streets should be the focus for commercial development in the neighborhood.
- The priority for *Community Connector* streets is to ensure adequate circulation through neighborhoods and to provide access to the city and regional road network. Development may include commercial, mixed-use or residential uses.
- *Neighborhood Connector* streets provide links between community open spaces and may include more amenities, such as special lighting and signage than typical *Residential* streets which are primarily for local access.
- *Industrial* streets should support industrial development needs by providing adequate access and loading.

Recommendations

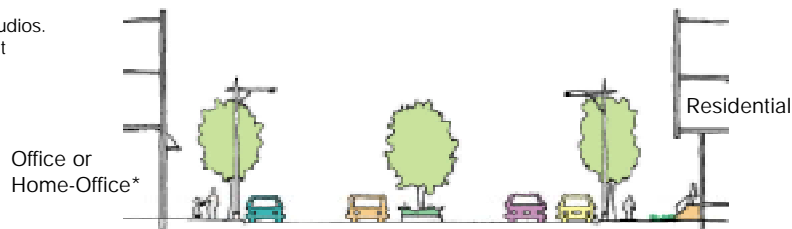
- Implement streetscape improvements that reflect the function of neighborhood streets and the type of development found on them. Street design elements include roadway widths, sidewalk widths, street lighting, street furniture, bus shelters and development uses.
- Traffic management techniques, such as traffic light synchronization, addition of stop signs, left turn lanes and traffic calming should be incorporated with the design of streetscape improvements.



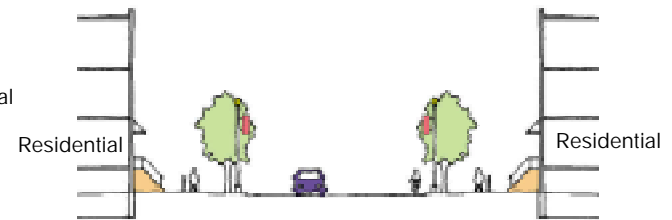
Community Connector - Western Avenue



Community Commercial - Chicago Avenue, Grand Avenue



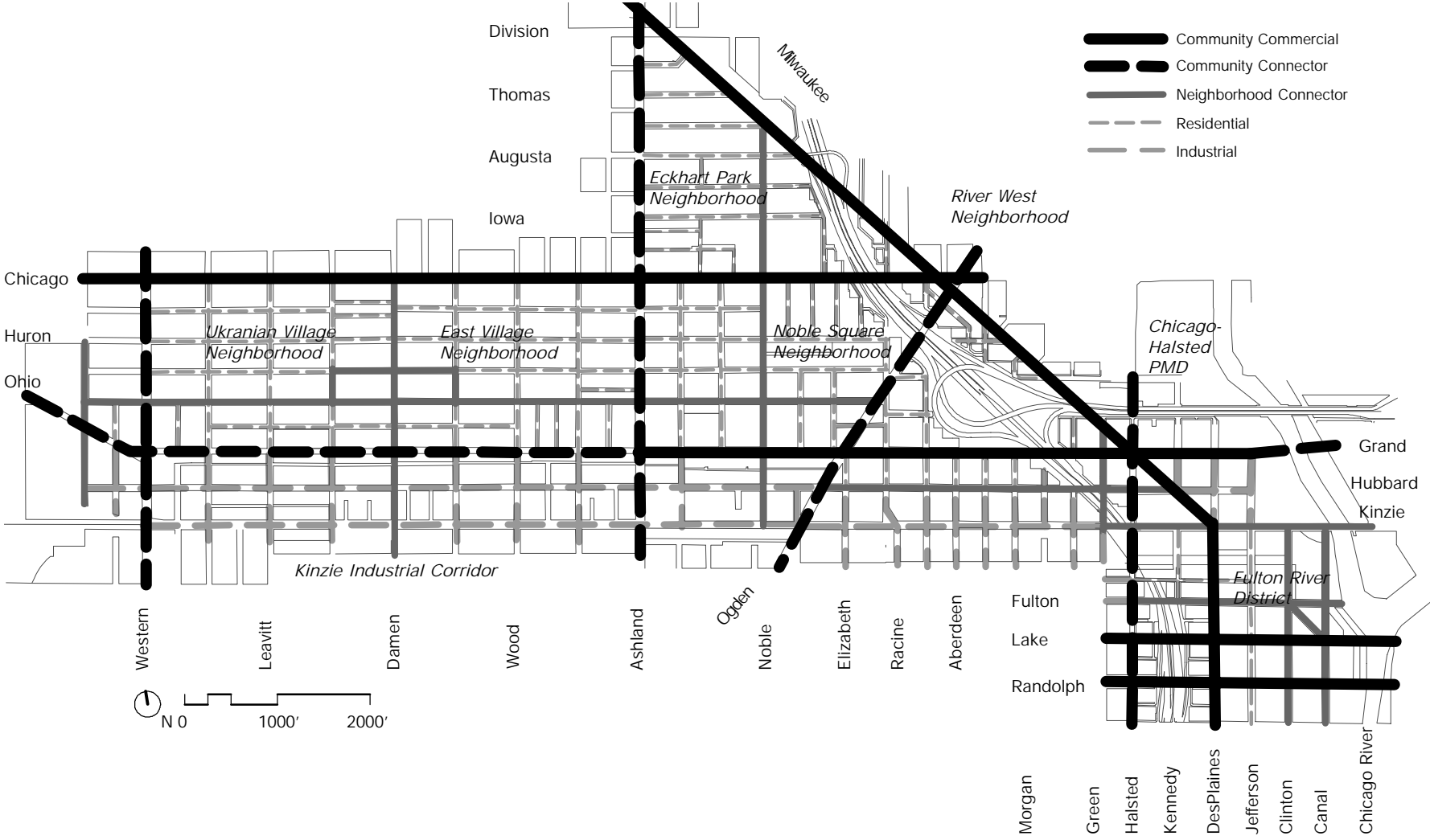
Community Connector - Ashland Avenue, Ogden Avenue



Neighborhood Connectors - Ohio Street, Noble Street

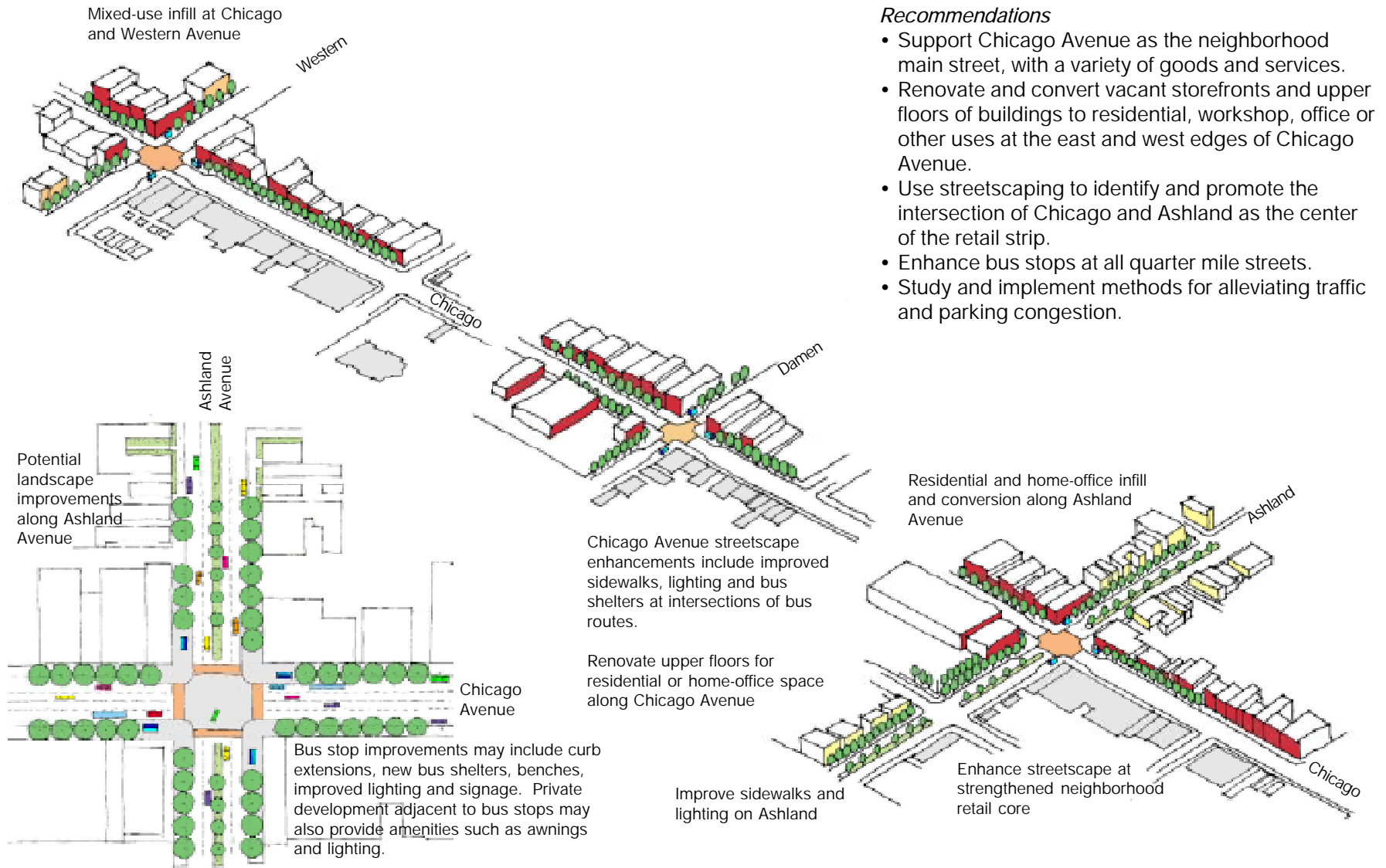
* Home-office spaces incorporate residential uses (bedrooms, kitchens, etc) with occupational uses such as office and studios. When home-office space is developed at street level, the work space should be oriented toward the street frontage.

Street Character Framework



Near Northwest Side

Chicago Avenue Revitalization
The Neighborhood's Main Street



Recommendations

- Support Chicago Avenue as the neighborhood main street, with a variety of goods and services.
- Renovate and convert vacant storefronts and upper floors of buildings to residential, workshop, office or other uses at the east and west edges of Chicago Avenue.
- Use streetscaping to identify and promote the intersection of Chicago and Ashland as the center of the retail strip.
- Enhance bus stops at all quarter mile streets.
- Study and implement methods for alleviating traffic and parking congestion.

Potential Streetscape Enhancements at Intersections

Milwaukee Avenue Revitalization
A Diverse Neighborhood Commercial Street

Recommendations

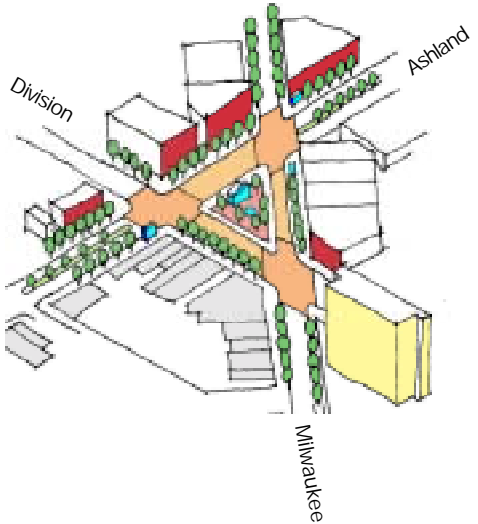
- Encourage mixed-use development along Milwaukee Avenue. Consolidate convenience retail at Division Street, Chicago Avenue, and Grand Avenue adjacent to CTA Rapid Transit Stations and bus transfer points.
- Improve visibility of CTA Rapid Transit Station entrances through new shelters, improved signage, lighting and public art.
- Enhance bus stops at all quarter mile streets.
- Improve the Milwaukee Avenue streetscape. Streetscape improvements may include lighting, benches, trash receptacles, benches, special paving and public art.



Milwaukee Avenue at Rapid Transit Stops



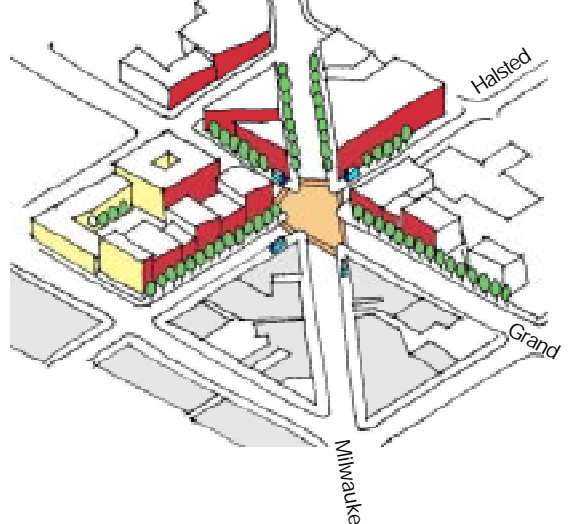
Milwaukee Avenue at locations without Rapid Transit stops



Milwaukee, Ashland and Division Intersection
Strengthen existing retail at enhanced transit plaza intersection



Milwaukee, Chicago and Ogden Avenues Intersection
Promote new retail at enhanced transit plaza intersection

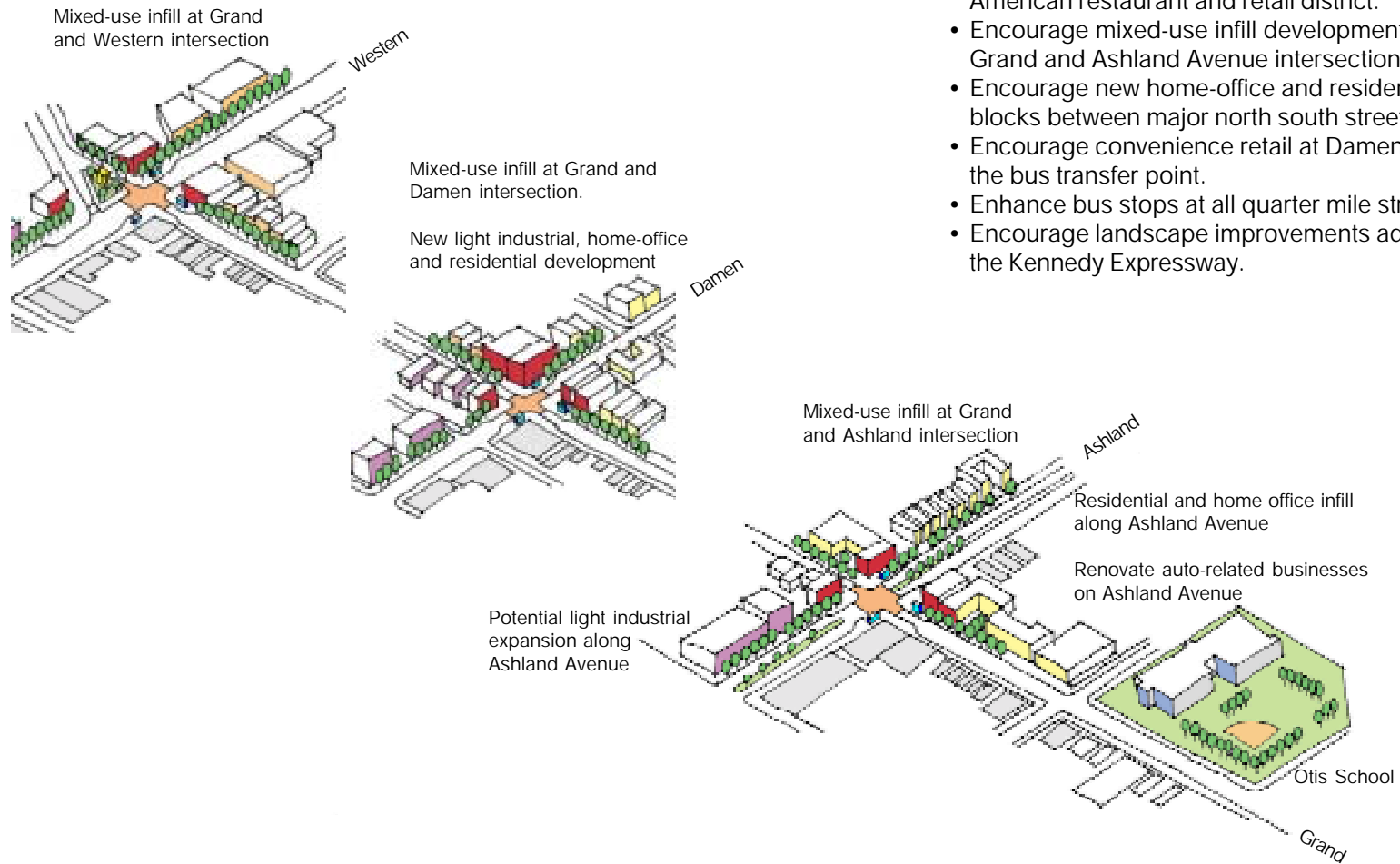


Milwaukee, Grand and Halsted Intersection
Reinforce existing character of area with new dining and retail establishments.

Grand Avenue Revitalization
Neighborhood Transit Development
and Restaurant District

Recommendations

- Improve streetscaping to increase visibility of Italian American restaurant and retail district.
- Encourage mixed-use infill development at the Grand and Ashland Avenue intersection.
- Encourage new home-office and residential uses on blocks between major north south streets.
- Encourage convenience retail at Damen Avenue at the bus transfer point.
- Enhance bus stops at all quarter mile streets.
- Encourage landscape improvements adjacent to the Kennedy Expressway.



Ashland Avenue

Recommendations

- Focus retail at the major intersections with Grand and Chicago Avenues and Division Street.
- Encourage residential and home-office infill north of Grand Avenue to Chicago Avenue at inactive business locations.
- Improve sidewalk conditions, minimize curb cuts and encourage parking lot landscaping, lighting and fencing improvements.
- Renovate and convert vacant storefronts and upper floors of buildings to other uses north of Chicago Avenue to Milwaukee.
- Enhance bus stops at all quarter mile streets.

Western Avenue

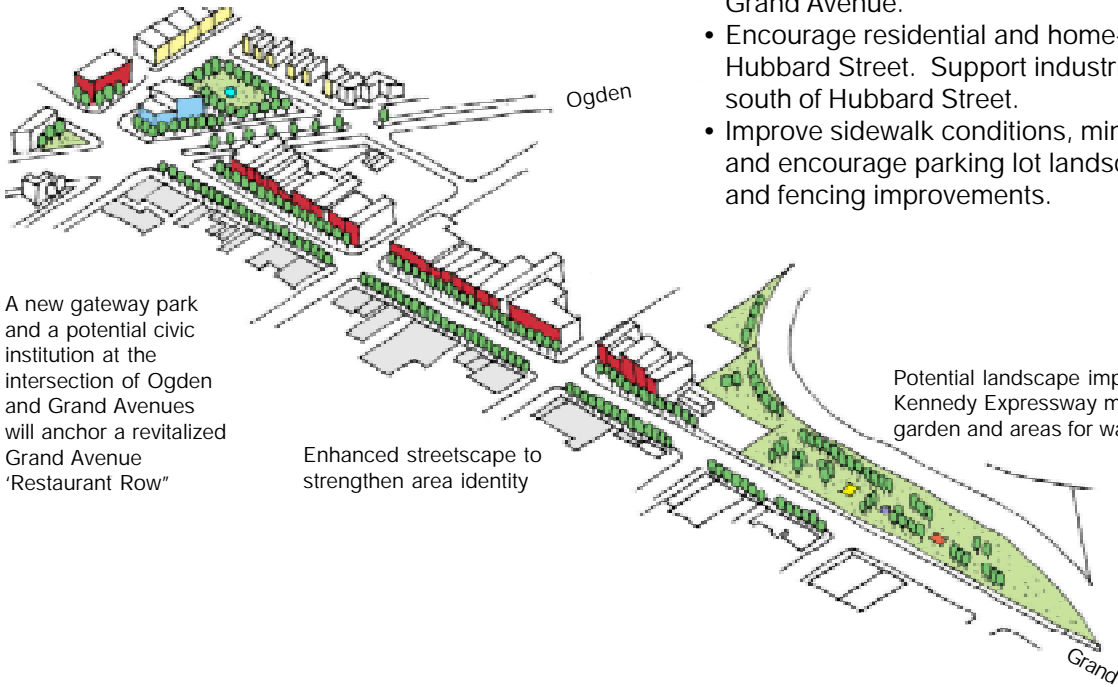
Recommendations

- Focus retail at the major intersections with Chicago and Grand Avenues.
- Add bus shelters at all quarter mile streets and at Hubbard to encourage transfers to and from Metra.
- Encourage residential and home-office infill north of Grand Avenue to Chicago Avenue. Support industrial development south of Grand Avenue.
- Improve sidewalk conditions, minimize curb cuts and encourage parking lot landscaping, lighting and fencing improvements.

Ogden Avenue

Recommendations

- Focus retail at the major intersections with Grand and Milwaukee Avenues.
- Create a new gateway park at Ohio Street and Grand Avenue.
- Encourage residential and home-office infill north of Hubbard Street. Support industrial development south of Hubbard Street.
- Improve sidewalk conditions, minimize curb cuts and encourage parking lot landscaping, lighting and fencing improvements.



A new gateway park and a potential civic institution at the intersection of Ogden and Grand Avenues will anchor a revitalized Grand Avenue 'Restaurant Row'

Enhanced streetscape to strengthen area identity

Potential landscape improvements along Kennedy Expressway may include a sculpture garden and areas for walking dogs

Strengthening Neighborhood Corridors

Design Recommendations



Contextual design principles, such as maintaining the streetwall, offering a mix of uses and at-grade storefronts can be applied to contemporary architecture.

Reflecting the Context

New infill development and building renovations should be of high architectural quality and be sensitive to the historic context in which it is occurring. New buildings should reflect the scale, orientation and character of existing surrounding buildings on the street. By following some simple guidelines, new buildings can become vibrant, contemporary additions to commercial corridors, while reflecting the positive qualities so valued by earlier generations.



Access to parking and service should be oriented to the alley or the secondary street.

Uses along Commercial Corridors

Commercial corridor buildings may have retailing, restaurant, office, workshop or residential uses. It is likely that many buildings will have a mix of uses, with residential on upper floors. Regardless of the overall use, the ground floor should always have active uses fronting the street.



Building articulation can help enhance the pedestrian environment.

Building setbacks

Buildings should be built to the property line or reflect the typical setback established for the block as a whole. Most buildings on local corridors are built to the property line. However, some residential buildings are set back five or ten feet. Where new residential buildings are constructed, they may follow this pattern if they neighbor such historic examples. Retail and office buildings should be built to the property line. If there is an existing streetwall along the block, it should be maintained, avoiding upper floor setbacks.

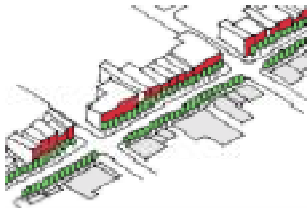
Ground Floor Flexibility and Transparency

Many corridor buildings have been homes, storefronts or workshops over the course of their life. To allow for future adaptation in case of the change of uses, the ground floor of new mixed use buildings should be designed to allow the conversion from retail or office to residential and vice versa. Ground level floor-to-ceiling heights should be a minimum of 14 feet. The ground floor facades should be transparent along the commercial corridors regardless of use.

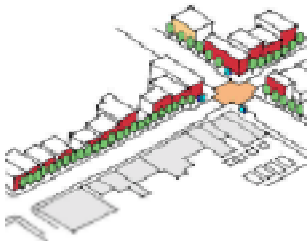
Parking and Service Access

Parking and service access should be at the rear of buildings off the alley and should not front commercial corridors. Access from secondary streets may be considered, where no alley exists. If interior parking is required, it should be recessed to allow active uses at the ground floor facing the commercial corridor.

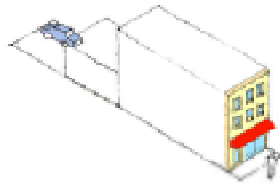
Design Recommendations for Neighborhood Corridors



District Commercial Corridors - Infill Development
Higher-density mixed-use buildings along the commercial corridors



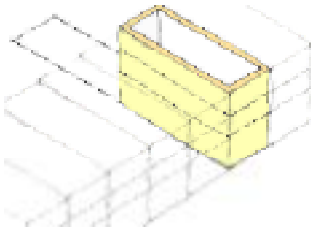
District Commercial Corridors - Intersection Development
Higher-density mixed-use, multi-family and corner buildings



Building Orientation and Lot Access

The front door should face the commercial corridor.

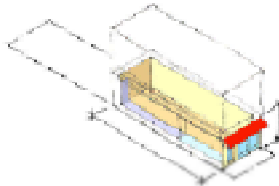
Vehicle and service access should be from the alley.



Building Massing

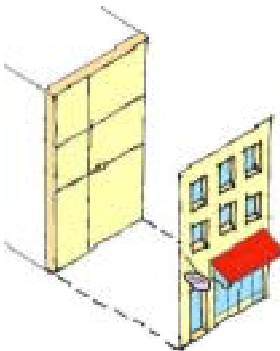
Maintain existing streetwall and continuity of streetfrontage.

Building height, width and depth should reflect the scale and pattern on the block. Development should be reviewed within the context of the adjacent buildings.



Ground Floor Flexibility and Transparency

Ground level floor-to-ceiling heights should be a minimum of 14 feet, the supporting structure and the mechanical system should allow for flexibility. ADA accessibility should be ensured. Ground floor facades along a commercial corridor should be transparent, and potentially feature display windows.



Façade Composition and Articulation

A pleasant arrangement of doors, windows and awnings along with attractive facade articulation can enhance the pedestrian realm.

Promoting Livable Residential Neighborhoods

Recommendations

Building Orientation and Lot Access

- The front door or main entry to all residential buildings should face the street.
- Parking should be accessed from the alley.
- Surface-parking lots, garages and corresponding curb cuts along public streets are discouraged.
- Parking and trash receptacles should be located at the rear of properties, preferably along alleyways, and shielded from view with landscaping or fencing.
- Loading facilities should also be designed for rear, alley access.

Scale and Massing of New Development

- Development should respect the scale of the adjacent properties.
- Development should maintain the existing streetwall and building setback along the street.
- The Fulton River District generally consists of low and mid-rise (four to twelve story) masonry warehouse buildings from the late 19th and early 20th Century. New development in the Fulton River neighborhood should respect the scale of the existing buildings. The existing zoning regulations for a C3-5 district and a F.A.R. of seven should be maintained. Scale will vary among developments, and taller buildings are appropriate within the Fulton River District especially along the Chicago River and adjacent to transit where higher density development is appropriate. Mid-rise buildings in the Fulton River District should be built to the property line to maintain the area's historic street walls.

- Development should respect the historic scale of the Eckhart Park, Noble Square, East Village and Ukrainian Village by building single family, two and three flat buildings. Larger buildings are appropriate at corners, but they may not exceed the height limits recently adopted for R-3, R-4 and R-5 districts.
- Development along the corridors may not exceed the height limits recently adopted for B-1, B-2, B-3, C-1, C-2 and C-3 zones.
- Front and side setback for bungalows, single family homes, townhomes, two-flat and three-flat buildings should reflect the typical setback alignments for the blocks on which they are located.

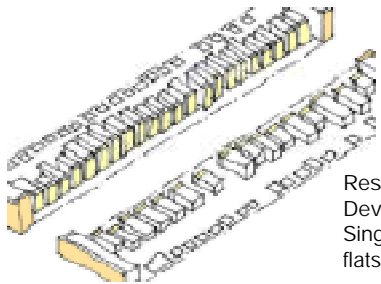
Façade Composition and Articulation

- Façade composition and building articulation should reflect its specific block and site context.
- Exterior street level features such as entrances, windows, awnings and architectural ornamentation should be included and scaled appropriately to the pedestrian environment. Visible blank walls are discouraged.
- Porches, bay windows and corner windows are all encouraged.



Infill development projects should reflect the scale and character of the existing building pattern on the block. The street wall should be maintained. The height and scale of buildings should average the existing building massing around new developments. Existing patterns established by floor heights, openings and materials should be reflected in new infill development.

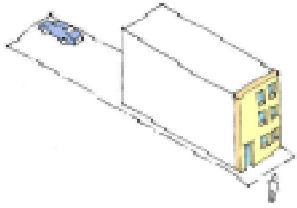
Eckhart Park, Noble Square, East Village and Ukrainian Village



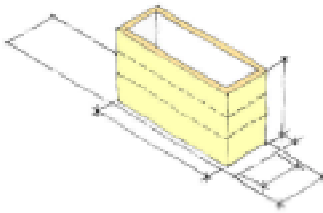
Residential Block Development
Single-family, two- and three-flats on neighborhood streets



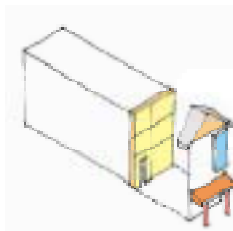
Residential Blocks - Intersection Development
Higher-density multi-family and mixed-use corner buildings



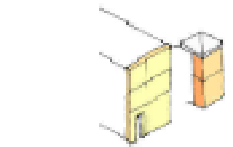
Building Orientation and Lot Access
Front door should face the street
Vehicle access should be from the alley



Building Massing
Maintain existing streetwall
Height should not exceed the average number of storeys on the block or as allowed by zoning



Lot widths, building setback, width and depth should reflect the development patterns on the block

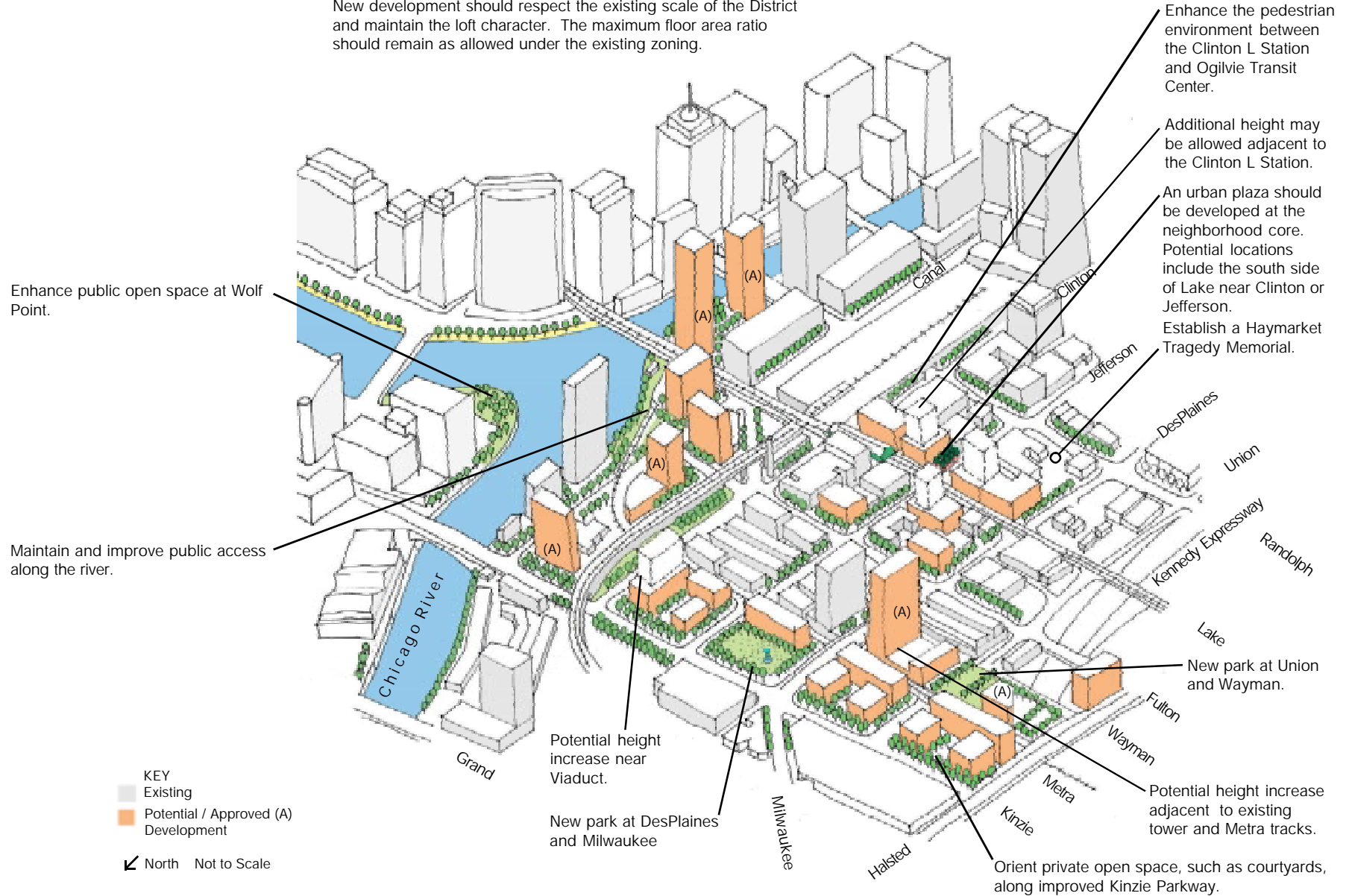


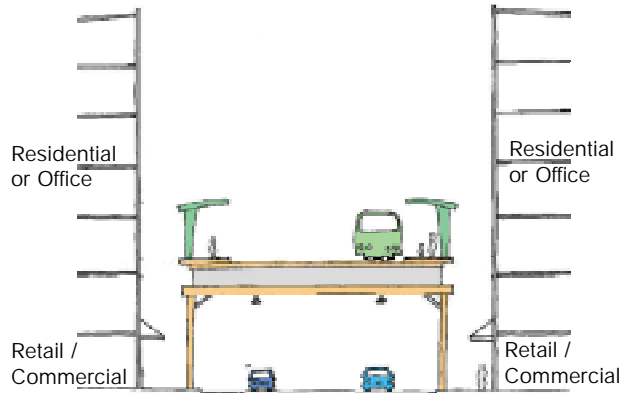
Façade Composition and Articulation
Specific residential design guidelines may be developed to address issues such as façade composition, front porches, and bay windows. Additional guidelines for corner lot developments may address potential bay projections

Promoting Livable Residential Neighborhoods

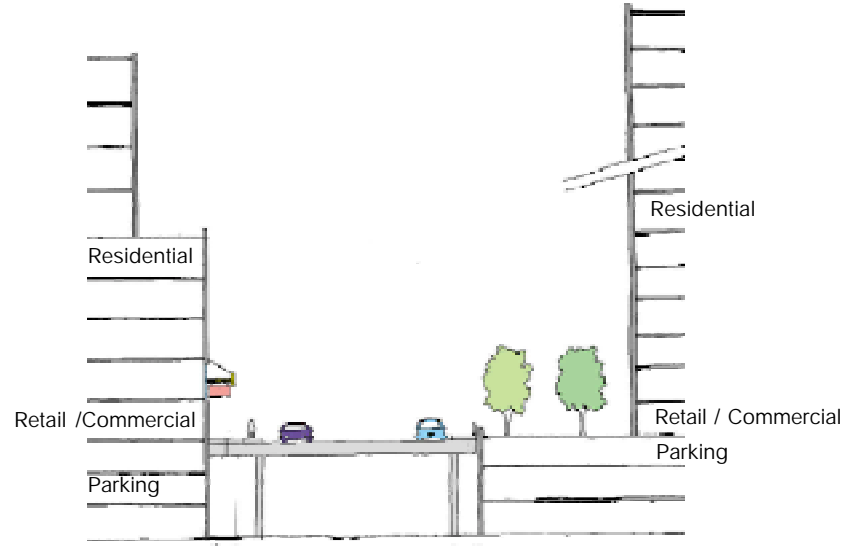
The Fulton River District

New development should respect the existing scale of the District and maintain the loft character. The maximum floor area ratio should remain as allowed under the existing zoning.

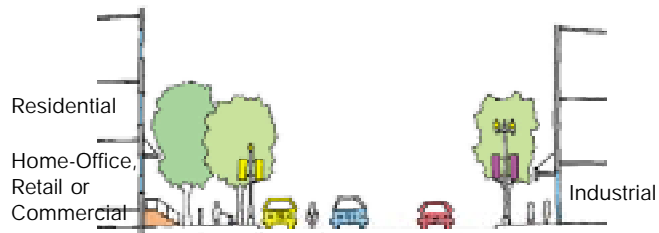




Lake Street



DesPlaines Street



Kinzie Street



Next Steps

Implementing the Plan

Next Steps

Implementing the Plan

The time line for implementation of this plan is twenty years. Implementation will require the partnership of the City of Chicago's Department of Planning and Development, Department of Transportation, the Chicago Park District and Chicago Public Schools. Neighborhood residents, elected officials and local organizations will also be critical partners.

Implementation will require the promotion of neighborhood corridors as places for new development, investment by City of Chicago departments and agencies, and the guidance of new development as it occurs.

The tables on pages 3-3 to 3-5 outline major activities, time frames, potential partners and potential funding sources for implementing the plan's recommendations. Additional information on potential implementation and funding tools is provided on pages 3-6 and 3-7.

Many of these activities can be incorporated into the City's ongoing capital investment and development regulation activities. Others will require more directed initiatives, such as investment in new parks and open spaces and new transit facilities. Recommendations on new uses, densities and design requirements must feed directly into the efforts of the Mayor's Zoning Reform Commission.

Further Planning

This plan has not addressed all issues faced on the Near Northwest Side. Additional efforts are recommended to respond to calls to reduce the impact of traffic. A comprehensive traffic and parking study should be developed for the community. Retail parking must also be managed in neighborhood shopping areas where store employees, delivery truck and customers often jostle for space.

The Chicago Department of Transportation is encouraged to take on traffic and parking issues as part of the Chicago Central Area Transportation Plan. The Zoning Reform Commission is also encouraged to respond to issues relating to parking for neighborhood retail districts, where local organizations and elected officials can also help develop parking implementation strategies.

This document is intended to be a working document over the twenty year life of the plan. Whereas some recommendations may not come to fruition, new opportunities may arise and future recommendations should be reviewed within the framework of this plan.

Open Space

Activities	Time Frame			Potential Partners	Potential Funding
	In Progress	1 - 5 Years	6 - 20 Years		
School Campus Greening - Talcott School - Otis School - Carpenter School - Mitchell School - Peabody School - Wells School	Completed Completed	● ●	● ● ● ●	CPS DPD CPD	City - CitySpace Open Space Impact Fee
Coordinate Development of New Public Open Space - Ogden Gateway Park (Ogden & Grand) - Kinzie Park (Kinzie & Des Plaines) - Wayman Park (Union & Wayman) - Urban Plaza (Lake & Clinton or Jefferson) - Riverwalk Greenway	● ●	● ● ● ●	● ● ●	DPD CPD Property Owners Neighborhood Organization	City - TIF CitySpace NeighborSpace Open Space Impact Fee Private
Develop Bike Routes	●	●		DPD CDOT	Fed State City - TIF
Develop Linear Open Space along Rail Viaducts - Hubbard Street - Clinton Street - Kinzie Rail Connector		● ●		Union Pacific DPD, CPD Open Space Organizations Arts Organizations Metra	Fed State Union Pacific City Private
Develop Plan for Pocket Parks		●		DPD (NeighborSpace) Neighborhood Organizations	City - CitySpace NeighborSpace Private

Key to Abbreviations:

Potential Partners:

- CDOT Chicago Department of Transportation
- CTA Chicago Transit Authority
- IDOT Illinois Department of Transportation
- CPD Chicago Park District
- CPS Chicago Public Schools
- DOZ Department of Zoning
- DPD Department of Planning and Development

Potential Funding:

- Fed Example: TEA-21 (Transportation Equity Act for the 21st Century)
- State Example: Illinois First Funding
- City Examples: CIP Capital Improvement Program
EZ Enterprise Zone
SSA Special Service Area
TIF Tax Increment Financing
CitySpace Program
NeighborSpace Program
Open Space Impact Fee Program
- Private Impact Fees and P.D. Negotiation Concessions

Transportation and Infrastructure

Activities	Time Frame			Potential Partners	Potential Funding
	In Progress	1 - 5 Years	6 - 20 Years		
Develop a Comprehensive Traffic and Parking Plan - Fulton River District - Milwaukee Avenue - Halsted Street - Chicago Avenue - Grand Avenue		● ●		CDOT DPD Elected Officials Property Owners Neighborhood Organizations	City - TIF Private
CTA Rail Improvements - New Green Line Stations - Enhanced Blue Line Entries		● ● ●	● ● ●	CTA	Fed State City
Improve pedestrian environment on Clinton Street between CTA station and Ogilvie Transportation Center		●		Union Pacific CTA CDOT	Fed State City - CIP, TIF Private
CTA Bus Stop Improvements		●		CTA, CDOT Neighborhood Organizations	Fed State City
Streetscape Improvements - Milwaukee Avenue - Grand Avenue - Chicago Avenue - Ohio and Noble Street	● ●	● ● ● ●		CDOT DPD Property Owners Neighborhood Organizations	Fed State City - CIP, TIF, SSA, EZ CitySpace
Expressway Landscaping	●	●		IDOT Chicago Gateway Green Corporate Organizations Foundations	Fed State City Private
Chicago's Central Area Plan projects - Clinton Street Subway - Clinton Street Busway - Carroll Avenue Busway			● ● ●	USDOT IDOT CDOT, DPD, CTA METRA, Amtrak, Union Pacific	Fed State City Private

Key to Abbreviations:

Potential Partners:

CDOT Chicago Department of Transportation
 CTA Chicago Transit Authority
 IDOT Illinois Department of Transportation
 CPD Chicago Park District
 CPS Chicago Public Schools
 DOZ Department of Zoning
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Potential Funding:

Fed Example: TEA-21 (Transportation Equity Act for the 21st Century)
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Land Use Development

Activities	Time Frame			Potential Partners	Potential Funding
	In Progress	1 - 5 Years	6 - 20 Years		
Coordinate with Zoning Reform Commission: - Land Use & Density Regulations	●	●		Zoning Reform Commission DOZ Elected Officials DPD Property Owners Neighborhood Organizations	N/A
Retail Corridor Strategy	●	●	●	DPD Elected Officials Property Owners Resident Organizations Chambers of Commerce	City - TIF Private
Residential Development	●	●	●	DPD Property Owners	City - TIF Private
Civic and Institutional - Ogden Gateway Project (Ogden & Grand)		●		DPD Neighborhood Organizations	City Private
Industrial Development	●	●	●	DPD Local Industrial Council	City - TIF EZ
Historic Preservation - Landmark Preservation	●	●	●	DPD, Landmarks Division	Property Tax Incentives, Income Tax Credits and Permit Waivers for designated buildings
Haymarket Tragedy Memorial		●		DCA	City of Chicago and Civic Organizations

Key to Abbreviations:

Potential Partners:

CDOT Chicago Department of Transportation
CTA Chicago Transit Authority
IDOT Illinois Department of Transportation
CPD Chicago Park District
CPS Chicago Public Schools
DOZ Department of Zoning
DPD Department of Planning and Development
DCA Department of Cultural Affairs

Potential Funding:

Fed Example: TEA-21 (Transportation Equity Act for the 21st Century)
State Example: Illinois First Funding
City Examples: CIP Capital Improvement Program
EZ Enterprise Zone
SSA Special Service Area
TIF Tax Increment Financing
CitySpace Program
NeighborSpace Program
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Summary of Potential Implementation Tools

Capital Improvement Program (CIP)

The CIP identifies the physical facilities and improvements planned for the City to support and enhance its neighborhoods, stimulate the economy, and make City services more efficient. Capital projects are permanent “bricks and mortar” improvements to the City’s physical infrastructure and to City-owned buildings. Examples of capital improvement projects are construction of new branch libraries, rehabilitation of bridges, repaving of streets and alleys, and the replacement of deteriorated sewer and water mains. New construction may be required to accommodate increased demand, while existing infrastructure requires periodic rehabilitation or replacement to protect the City’s earlier capital investment.

Planning for capital improvements is an ongoing process; as the City’s infrastructure ages, and as needs change, capital programs and priorities must be adjusted. Each year, City departments submit their projected capital needs for the next five years. Public hearings are held in late Fall to inform citizens about the capital selection process and to obtain additional public input. Aldermen are also given the list of proposed capital projects for review and comment. As with citizens, the aldermen make recommendations on the proposed projects or submit requests for new capital projects. The Office of Budget and Management (OBM) then meets with each department to examine the merits of their capital requests. Recommendations are incorporated into a draft CIP which is reviewed by the Capital Improvement Advisory Committee. The final Capital Improvement Program is presented to the Mayor for approval and then released to the City Council and the citizens of Chicago.

CTA Adopt-A-Station Program

The Adopt-A-Station Program is an initiative for community groups or businesses to “adopt” stations and take responsibility for improving the physical condition of rail stations/sites.

Chicago Gateway Green Partnership

The Partnership is a nonprofit corporation dedicated to the beautification of Chicago area expressways including litter removal, landscape plantings and installation of participant recognition signs.

CitySpace Program

The City Space Program is an intergovernmental initiative including the City, Chicago Park District, and Cook County Forest Preserve District to implement the CitySpace Plan. Program initiatives address improvements to school parks, greenway planning, neighborhood parks and streetscapes.

Enterprise Zone Program

The objectives of the City of Chicago Enterprise Zones are to stimulate economic activity and revitalize declining neighborhoods. The Program provides State and City incentives and assistance to encourage the retention and expansion of businesses in the City’s six designated Enterprise Zones including:

- Sales Tax Exemption
- Property Tax Reduction
- Finance Assistance
- Real Estate Tax Exemption
- Investment Tax Credit
- State Jobs Creation Credit
- Machinery and Equipment Sales Tax Exemption
- Utility Tax Exemption.

Facade Rebate Program

The City's Facade Rebate Program is administered by DPD and provides rebates for building rehabilitation such as facade renovation, exterior lighting, signage/graphics, windows and display, security and energy conservation systems.

Micro Loan Program

The City's Micro Loan Program provides business loans to eligible small businesses which create jobs for low or moderate income City residents. The funds may be used for machinery, equipment, renovation or working capital.

NeighborSpace Program

The NeighborSpace Program is a nonprofit corporation created by the City, Chicago Park District, and Cook County Forest Preserve District to develop and support small neighborhood open spaces such as community gardens, mini-parks, greenways, plazas and industrial area landscapes.

Open Space Impact Fee Program

The Open Space Impact Fee Program requires new residential development to contribute to a pool of money for expanding the supply and capacity of public open space within the community area. Permitted uses of the fund include: acquisition/development of new parks and development of lands surrounding public schools to provide landscaping, playground equipment, sidewalks, recreation areas and park furnishings.

Special Service Area Financing (SSA)

The SSA program is a state-authorized financing program administered by the City and funded through a separate property tax levy paid by property owners within the designated area to support local improvements.

Tax Increment Financing (TIF)

TIF is a state authorized program administered by the City that provides targeted financial resources for a specific redevelopment area. Projects eligible for funding include property acquisition, demolition, infrastructure improvements, certain financing costs, relocation, job training and environmental remediation.

Resources for Historic Landmarks

Financial benefits from federal and local programs include: reduced property tax assessments for commercial rehab (Class L), income tax credits for commercial rehab and for easement donations, building permit fee waivers, property tax freezes for residential rehabilitation, building and zoning code flexibility and technical assistance from City preservation specialists.

Acknowledgements

The Near Northwest Side Planning Advisory Committee

Alderman Walter Burnett, Jr.	27 th Ward
Alderman Jesse Granato	1 st Ward
Alderman Theodore Matlak	32 nd Ward
Alderman Burton Natarus	42 nd Ward
Chuck Abraham	Metra
John Bosca	Neighbors of River West
Randall Buescher	Neighbors of River West
Carl Byrd	Chicago Department of Transportation
Kathy Dickhut	Chicago Department of Planning and Development
David Eisenberg	Eckhart Park Community Council
Gail Filkowski	West Loop Gate
Jonathan Fine	East Village Association
Barbara Frankel	Fulton River District Association
Martha Goldstein	West Loop Gate
Don Grabowski	Chicago Department of Transportation
Jeff Hinkle	Chicago Transit Authority
Jim Horan	Industrial Council of Nearwest Chicago
Jean Jell	Ukrainian Village Association
John Joslin	Near Northwest Chamber of Commerce
Tim Later	Chicago Department of Transportation
Anthony Lucafo	Near Northwest Civic Committee
Mary Clare Maxwell	Fulton River District Association
Patrick McAtee	Metra
Brenda McGruder	Metra
Cyndi Saylor	West Central Association
Martin Stratton	Chicago Department of Planning and Development
Robert Wiggs	West Central Association

Project Staff

Nori Bleiman	Chicago Department of Planning and Development
Mary Bonome	Chicago Department of Planning and Development
Edel Perez-Campos	Chicago Department of Planning and Development

Project Consultants

Christopher Hall	Skidmore, Owings & Merrill LLP
Otto Condon	Skidmore, Owings & Merrill LLP
Lisa Willis-Brown	Urban Works Ltd
Scott Swearngen	Urban Works Ltd
Christine Williams	Goodman Williams Group

Public Meeting Summary

Advisory Committee Study Area Tours:
May 16 & 18, 2000

Advisory Committee Meetings:
June 8, 2000
June 27, 2000
July 26, 2000
September 26, 2000
October 18, 2000
June 26, 2001

Public Meetings
November 15, 2000 - West Central Association
November 16, 2000 - Fulton River District Neighborhood Association
December 4, 2000 - Neighbors of River West
December 5, 2000 - West Loop Gate
October 23, 2001- Eckhart Park Field House